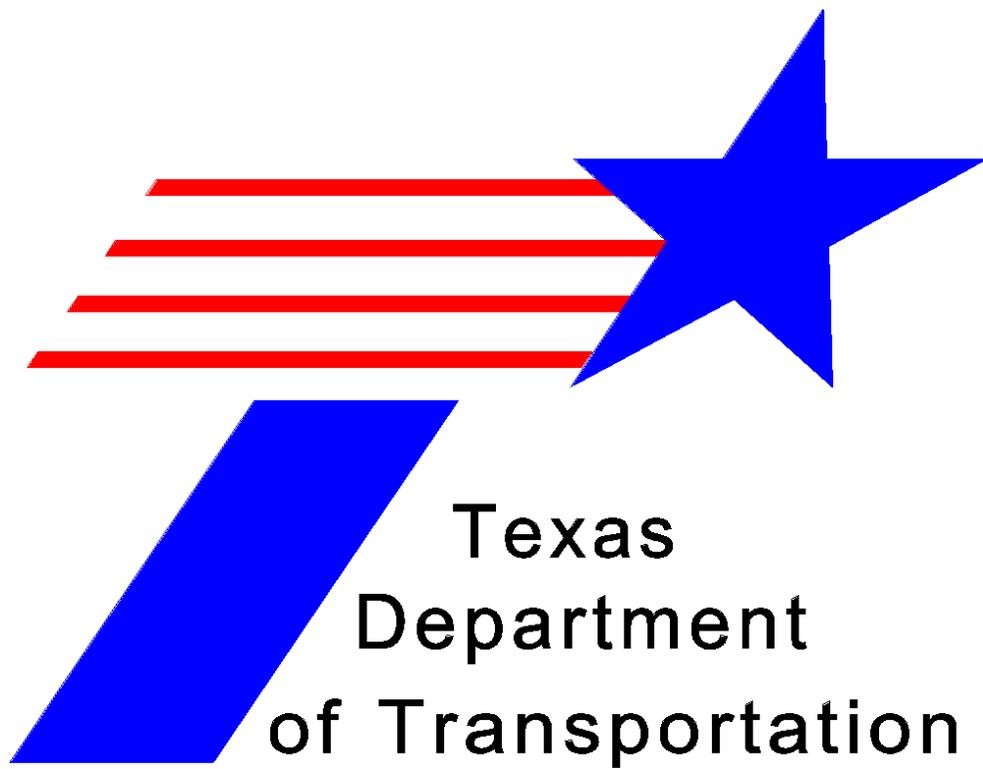


# Signs and Markings Manual



March 2006

## **Manual Notice 2006-1**

**From:** Carlos A. Lopez, P.E.  
Traffic Operations Division

**Manual:** *Signs and Markings Manual*  
(formerly the *Signs and Markings Volume* of the *Traffic Operations Manual*)

**Effective Date:** March 01, 2006

### **Purpose**

This manual revision includes the following new sections added to Chapter 7:

- ◆ Section 16, “Memorial Marker and Named Marker Highways and Structures” (formerly titled “Named and Memorialized Highways”)
- ◆ Section 17, “Historic Routes”
- ◆ Section 18, “Street-Named Highways”
- ◆ Section 19, “Historical Markers”

In addition the following changes have been made to Chapter 7:

- ◆ Section 4, “Signing for County Roads,” has been revised to reflect current signing standards.
- ◆ Section 9, “Traffic Generators, Special Events, and Government Offices,” has been revised with a new segment on the use of commercial names.
- ◆ Section 21 (formerly 19), “WELCOME TO TEXAS Signs,” has been revised to comply with Texas Transportation Code, Section 201.617.
- ◆ Section 22 (formerly 20), “Accessibility Signing for Rest and Picnic Areas,” has been revised to reflect current signing standards.
- ◆ Other minor technical updates have also been incorporated.

### **Review**

The Office of General Counsel and Audit Office both reviewed the draft version of this revision and found it adequate for its purpose.

## **Instructions and Content**

This manual notice includes a complete revision to Chapter 7.

In the print version of this manual (*Signs and Markings Volume of the Traffic Operations Manual*), replace the entirety of the main Table of Contents and Chapter 7 with this new content. Revised portions are marked with highlighting or, in the case of entire pages, vertical bars in the outside margins.

The on-line version of this manual now contains only this revised Chapter 7. For other content, see the print version.

## **Contact**

Address questions concerning information contained in this manual notice to Greg Brinkmeyer of the Traffic Operations Division, 512-416-3120.

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# Chapter 1

## Introduction

### Contents:

Section 1 — Overview

## **Section 1**

### **Overview**

NOTE: The on-line version of this manual currently contains only the latest revision of Chapter 7.  
For other content, see the print version.

# Chapter 2

## Sign Location and Installation

### Contents:

Section 1 — Overview

## **Section 1**

### **Overview**

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For other content, see the print version.

# Chapter 3

## Sign Materials and Supports

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## **Section 1**

### **Overview**

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For other content, see the print version.

# Chapter 4

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### Contents:

Section 1 — Overview

## **Section 1**

### **Overview**

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For other content, see the print version.

# Chapter 5

## Regulatory Signs

### Contents:

Section 1 — Overview

## **Section 1**

### **Overview**

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For other content, see the print version.

# Chapter 6

## Warning Signs

### Contents:

Section 1 — Overview

## **Section 1**

### **Overview**

NOTE: The on-line version of this manual currently contains only the latest revision of Chapter 7.  
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# Chapter 7

## Guide Signs

### Contents:

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[Section 3 — Oversize Street Name Signs](#)

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## Section 1

### Overview

#### Introduction

Chapters 2D and 2E of the *Texas Manual on Uniform Traffic Control Devices for Streets and Highways* ([TMUTCD](#)) provide detailed information on the application of highway guide signs. TxDOT's *Sign Crew Field Book* contains further detailed information on the types and uses of guide signs on conventional highways, including detailed discussions and illustrations of:

- ◆ guide sign components
- ◆ arrangement of guide signs on the sign assemblies
- ◆ placement of guide signs at intersection approaches and departures.

This chapter provides additional information and guidelines specific to TxDOT operations. Guide signing for freeways, expressways, and certain divided highways is covered in Chapter 9 of this manual.

The recommendations and guidelines contained in this chapter should be treated as general goals intended to improve the guidance of motorists; they are not necessarily applicable to every situation.

#### Purpose of Guide Signs

Guide signs are used to

- ◆ guide motorists along streets and highways
- ◆ inform motorists of intersecting routes
- ◆ direct motorists to streets, highways, cities, towns, villages, or other important destinations
- ◆ provide other information of value to road users.

Guide sign messages should generally be as simple and direct as possible.

#### Use of Brown Background

Brown background color should be used for recreational, cultural interest, and historical areas signing.

## Section 2

### Concurrent Routes

#### Avoiding Unnecessary Concurrent Routes

Concurrent routes should be avoided and their number reduced where possible.

**Example:** A state highway joins a US highway and runs concurrent with it into Town X. The US highway continues through the town, but the state highway ends at the town (see Figure 7-1). The concurrent signing should be eliminated, terminating the signing of the state highway where it joins the US highway.

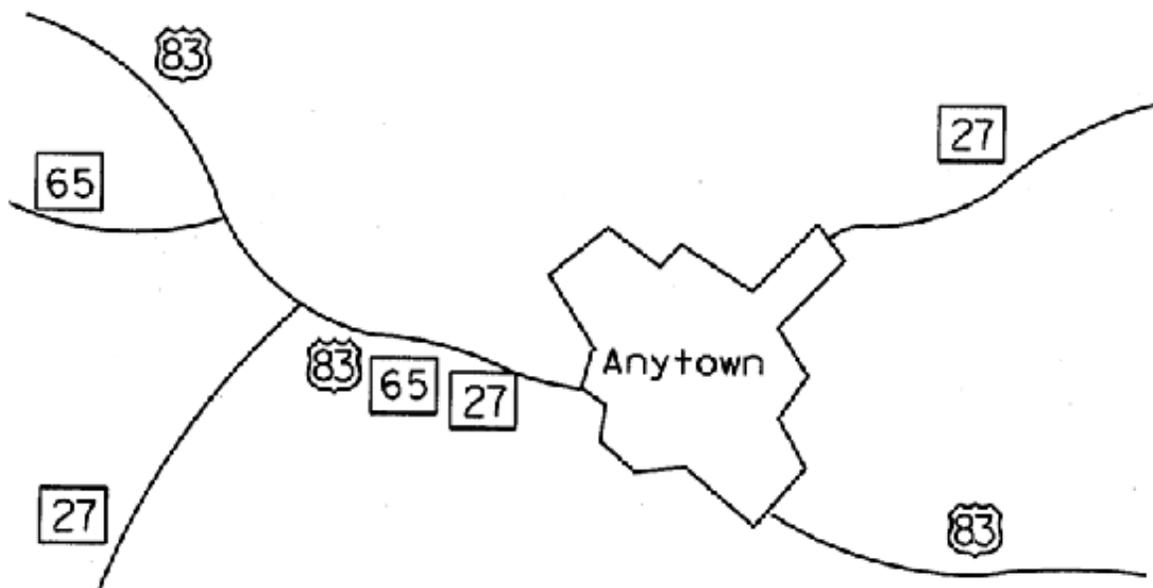


Figure 7-1. Example of unnecessary concurrent signing. Signing for SH 65 should terminate at its junction with US 83.

#### Concurrent Route Confirmation Assemblies

The Texas Reference Marker (TRM) system marks the highway so that all data collected and recorded identifies the correct section of road in the TxDOT data base. The Transportation Planning and Programming Division (TPP) has established a highway system hierarchy listing all classes of highways in the data base. On roadways having more than one route number assigned to them, it is very important that the route markers (M1 series) be mounted according to the roadway system hierarchy shown in Figure 7-2 to help data collectors record the proper highway number.

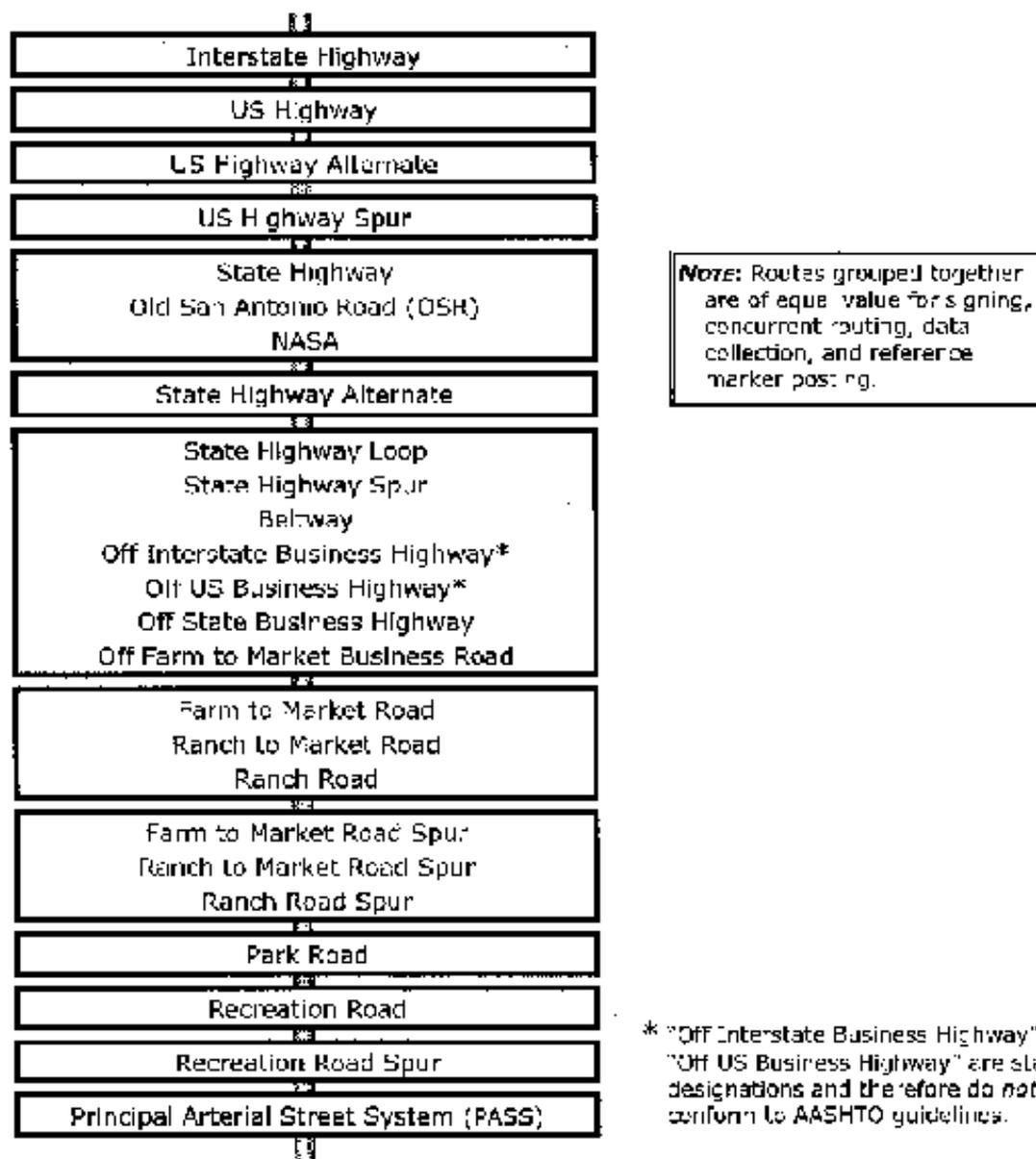


Figure 7-2. Roadway system hierarchy.

Markers for higher ranked systems are mounted either **above or to the left** of lower ranked systems.

**Example:** If a US and state highway are concurrent, the US shield is mounted above or to the left of the state highway shield.

## Section 3

### Oversize Street Name Signs

#### Recommended Use

Oversized or illuminated street name signs should be mounted on signalized intersection mast arms or span wires where possible.

#### Letter Size

The minimum recommended letter height for street names is 10 inch, except that 8 inches may be used where any of the following circumstances exist:s

- ◆ dual street names at intersections
- ◆ space limitations restrict the overall size of the sign
- ◆ roadway with speed limit of 30 miles per hour or less.

#### Dimensions

See *Standard Highway Sign Designs for Texas* for the [recommended dimensions](#) for oversized street name signs. See the [Traffic Control Standard Plan Sheets](#) for sheeting, font, and substrate requirements for oversize street name signs.

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## Section 4

### Signing for County Roads

#### Introduction

TxDOT furnishes and erects advance county road name or number signs on the state highway system. The guidelines contained in this section are intended to provide statewide uniformity.

#### Eligible County Roads

The following considerations govern the decision to install county road signs on state highways:

- ◆ the development of the 911 system and the importance of directing emergency vehicles
- ◆ the adequacy of any existing signing at night
- ◆ the need for directing non-local drivers.

Only county roads that have been officially assigned a name or route number by the commissioners court of a county qualify for signing.

#### Signing Standards

All signing for county roads on the state highway system use [I-2e series](#) signs (white on green), whether for named or numbered roads. The abbreviation for “county road” used on signs with road numbers is “CO RD.”

The recommended minimum letter size is 6 inches, except where circumstances restrict the overall size of the sign. Advance guide signs should be placed at least 200 feet in advance of the intersection. On major highways or for major county roads, larger letter size and increased advance distance from the county road should be considered. *Standard Highway Sign Designs for Texas* for [recommended dimensions](#) for county road signs. See the [Traffic Control Standard Plan Sheets](#) for sheeting, font, and substrate requirements for county road signs.

#### When Warning Signs are Used

At intersections with county roads where the W2 series intersection warning are used, the D1 series county road name or number sign may be used instead of the warning signs. Although the [TMUTCD](#) allows county road name signs to be mounted below the intersection warning signs, in order to be consistent, separate supports should be used if both warning and guide signs are erected for the same intersecting road.

### **Signs Provided by County**

Counties may provide county road name or number signs for placement similar to street names on top of supports of TxDOT maintained STOP and YIELD signs. Signs provided by the county for this purpose must:

- ◆ meet the design requirements of the TMUTCD
- ◆ be of the quality materials approved by the district engineer (including sign blanks, reflectorization, and mounting hardware)

### **Existing Signs**

All existing county road signing not in conformance with these guidelines may remain in place until the end of its service life. As existing signs are replaced, new signs will conform to established guidelines.

## Section 5

### Interstate Highway Numbering

#### Original Numbering System

The original numbering system for interstate highways established that:

- ◆ north-south routes end in 5, beginning with 5 on the west coast and increasing on routes east of IH-5
- ◆ east-west routes end in zero, beginning with 10 in the south and increasing on routes north of IH-10.

#### Current Numbering System

After the initial interstate routes were designated, additional corridors became eligible for interstate highway funding. Since the initial routes had used all the available numbers ending with “0” and “5,” an alternate numbering system was developed using even and odd numbers, as follows:

- ◆ Odd numbers designate routes that generally run in a north-south direction.
- ◆ Even numbers designate routes that generally run in an east-west direction.

The number selected would generally fall in the normal sequence between two existing interstate routes. For example, IH-27 is located between IH-25 (New Mexico, Colorado, Wyoming, etc.) and IH-35 (Texas, Oklahoma, Kansas, etc.).

#### Loops

Interstate loops use three digits; the first digit is an even number, while the last two digits are associated with the interstate highway that carries the major traffic stream.

**Business Loops.** Interstate business loops typically retain the number of the interstate highway traversing the city or town and use a green route sign.

**Interstate Loops in Texas.** Texas interstate loops (both existing and possible) are as follows:

**Interstate Loops in Texas**

Loop Number	Location
IH 210	El Paso (possible)
IH 235	Austin (possible)
IH 410	San Antonio
IH 435	Waco (possible)

**Interstate Loops in Texas**

<b>Loop Number</b>	<b>Location</b>
IH 610	Houston
IH 635	Dallas
IH 810	Beaumont (possible)
IH 820	Fort Worth

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## Section 6

### Special Route Markers

#### Hurricane Evacuation Route Signing (EM-1 series)

The Division of Emergency Management and the Department of Public Safety, acting through local disaster district committees, have established hurricane evacuation routes for each of the five coastal districts. The HURRICANE EVACUATION ROUTE sign (EM-1) with a hurricane symbol should be used to mark these routes. See *Standard Highway Sign Designs for Texas* for [sign details](#).

HURRICANE EVACUATION ROUTE signs should be installed at critical intersections along evacuation routes and any other appropriate locations between intersections. The sign should be mounted under the route marker for evacuation routes on the existing support, if possible. If a sign is independently mounted as a confirmation route or with a directional arrow or a directional marker, it should be installed on an approved breakaway support.

#### Texas Travel Trails

The Texas Travel Trails were officially established by the Texas Transportation Commission. The trails follow routes that have historical, cultural, or geographical significance.

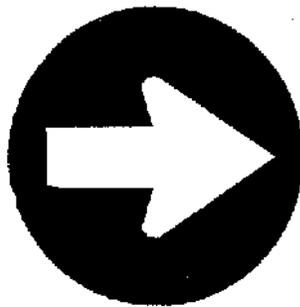
TxDOT erects and maintains the Texas Travel Trails signs. The signs are 42 × 24 inches. The trail arrows are 18 inches in diameter. Signs and trail arrows are obtained from the General Services Division's Regional Distribution Centers. Figure 7-3 shows an example of a Texas Travel Trail marker and associated directional arrow. See *Standard Highway Sign Designs for Texas* for [sign details](#).

Districts should maintain close observation of trail signing, and promptly obtain and erect replacements as needed to assure that the route remains well marked for motorists who are following the trail. Any problems or confusion about a particular trail route segment should be brought to the attention of the Travel and Information Division (TRV).

Existing designated Texas Travel Trails are:

- ◆ Texas Brazos Trail
- ◆ Texas Forest Trail
- ◆ Texas Forts Trail
- ◆ Texas Hill Country Trail
- ◆ Texas Independence Trail
- ◆ Texas Lakes Trail

- ◆ Texas Mountain Trail
- ◆ Texas Pecos Trail
- ◆ Texas Plains Trail
- ◆ Texas Tropical Trail.



*Figure 7-3. Example of Texas Travel Trail marker.  
(Trail arrow may be reversed for left turn.)*

## Section 7

### Destination and Distance Signing

#### Introduction

Destination signs (D1 series) provide guidance information in the form of a city name (destination) and the direction to the city. Destination signs are typically used in advance of intersections to help drivers decide which way to turn at the intersection.

Distance Signs (D2 series) indicate the distance to the city shown on the sign.

Destination and distance signs are especially valuable to motorists unfamiliar with a particular area. Because some motorists are inattentive, read maps poorly, or do not adequately plan their trips, additional destination and distance signs can help “pull” them through an area.

People familiar with an area often do not readily see a problem with destination signing. This section presents suggestions for handling situations that often present problems for motorists unfamiliar with an area.

Additional information on destination and distance signing can be found in the *Texas Manual on Uniform Traffic Control Devices* ([TMUTCD](#)) and the *Sign Crew Field Book*.

#### Combination of Routes Between Major Destinations

When traversing a combination of routes between major cities or destinations, motorists may find guide signing showing only route numbers confusing. Sometimes maps, with their limited detail, may also contribute to the disorientation.

Trailblazing signs for major cities or destinations placed between routes can help “pull” motorists following a combination of routes through an area. Figure 7-4 shows a typical situation.

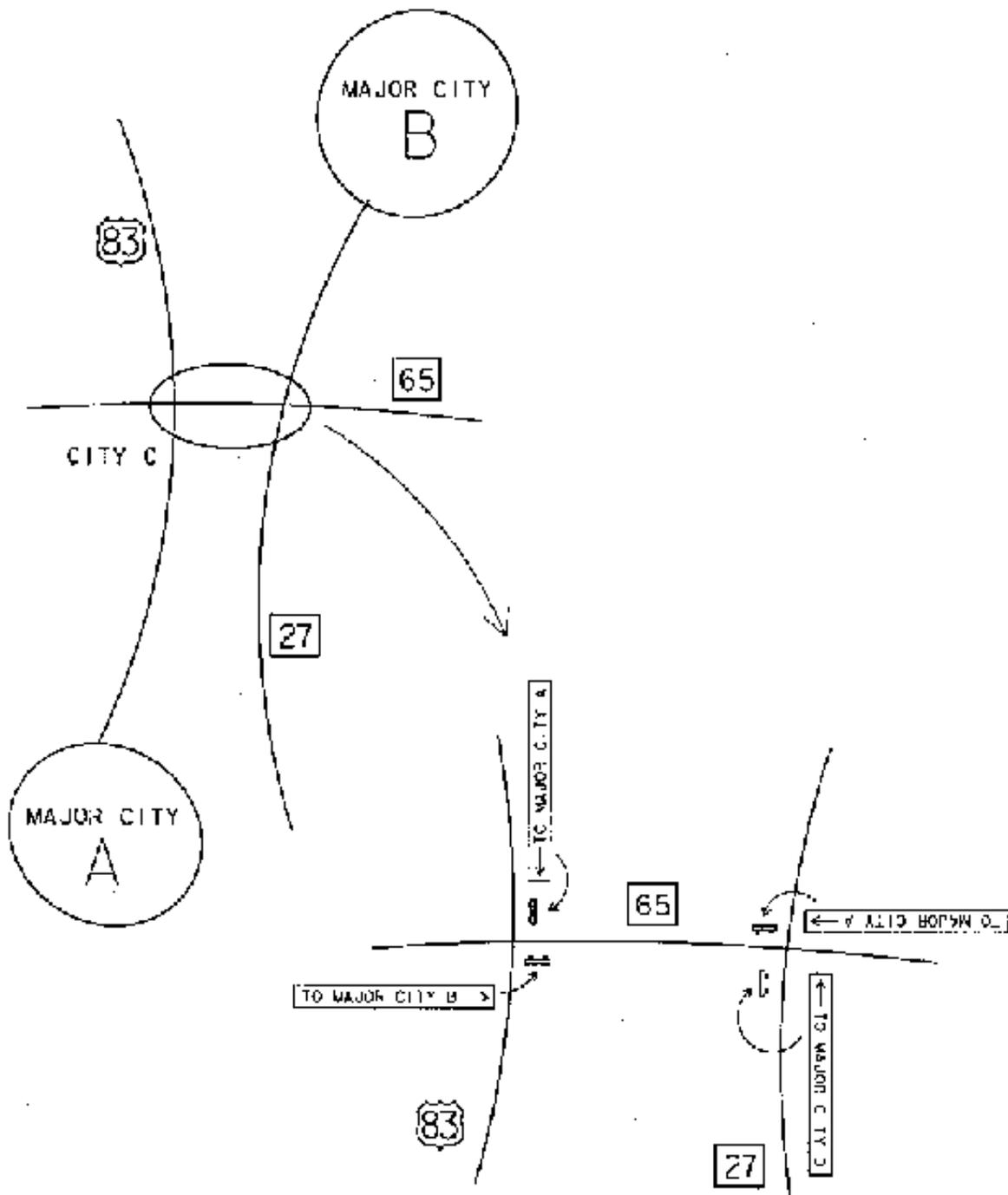


Figure 7-4. Because travel between City A and City B Requires a combination of routes, destination signing in City C helps pull motorists through.

### Additional Distance Signing

Distance signs are normally used to advise motorists of distances to destinations on a route. The destinations shown are selected based on the guidelines in the [TMUTCD](#), Section 2D-36.

Because many people navigate by destination names instead of route numbers, distance signing for other major destinations, in addition to that already in place, should be provided where applicable (subject to the maximum text permitted by the TMUTCD). Also, as previously discussed, trailblazing over a combination of routes for major destinations may be necessary if the routes together serve as a primary connecting link. [Figure 7-5](#) shows an example.

Because such distance signing and trailblazing expands the scope of normal distance signing, coordination with neighboring district offices is necessary.

### Spacing Additional Destination and Distance Signs

When additional destination and distance signs are required, the spacing between adjacent signs should be 300 feet minimum, with 400 feet desirable, on highways with 55 miles per hour or greater speed limits.

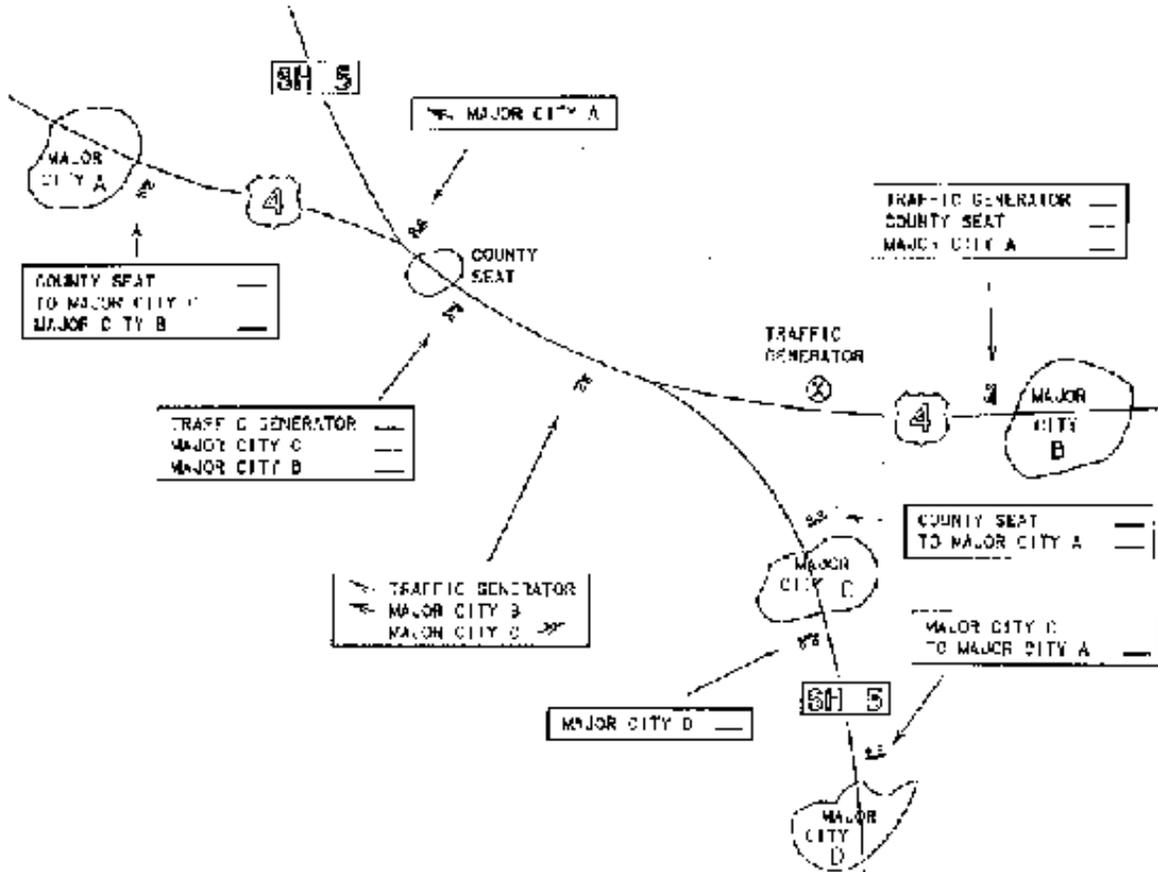


Figure 7-5. Example of supplemental destination distance signing.

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## Section 8

### Destination Signing for Border Cities in Mexico

#### Introduction

TxDOT provides destination and mileage signing for Mexican border cities and border crossings in a manner similar to that provided for Texas cities where sign size and spacing allow. Mexican border cities should be shown along with the corresponding Texas cities when appropriate.

#### Types of Border Crossings

There are two types of border crossings: “international” and “port of entry.”

**International crossings** are major crossings open 24 hours a day, 7 days a week

**Port of entry crossings** are minor crossings open fewer than 24 hours a day.

#### Sign Content

The destination signing should identify the city name or the type of crossing or both. If a city name is used without specifying the type of crossing, it implies that the crossing is an international crossing.

Destination signing for Mexican border cities should indicate the city names, but **not** the country name “Mexico,” except when necessary to prevent confusion.

Smaller cities with crossings should have destination and any necessary mileage signs at the nearest intersecting highway for the crossing roadway.

Where a Texas city is adjacent to a Mexican city, it may be desirable to sign both cities. For example, Eagle Pass and Piedras Negras might appear on the same sign because both cities are of similar importance.

#### Sign Placement

On US and state highways, destination signing for major border crossing cities should be placed at major intersecting highways in accordance with the *Texas Manual on Uniform Traffic Control Devices* ([TMUTCD](#)). On freeways and expressways, either advance guide and exit direction signs or supplemental signs may be used to sign major intersecting highway routes leading to border cities.

Where multiple routes to a destination exist in the same area, the best route for each direction of travel should be signed. Because routes to major border cities in Mexico may involve a combination of routes, additional destination signing may be appropriate. (See [Section 7](#) of this chapter.)

Normally, signing for border cities in Mexico should not extend more than about 50 miles from the Texas-Mexico border.

## Section 9

### Traffic Generators, Special Events, and Government Offices

#### Signing for Traffic Generators

Signing for traffic generators (D1, E1, and E2 series) and special events presents a challenge because of the wide variety of generators and their various sizes and locations in relation to highways. The *Texas Manual on Uniform Traffic Control Devices* ([TMUTCD](#)) contains a chart showing various generators and eligibility criteria for signing. Districts should follow these guidelines as closely as possible to maintain statewide consistency for signing requests.

#### Local Authority Requests for Traffic Generator Signs

Local authorities often request TxDOT to sign for various traffic generators. The following guidelines apply.

**Outside city limits** TxDOT erects signs for generators that meet the criteria established in Appendix D of the TMUTCD.

##### **Inside city limits:**

- ◆ TxDOT erects signs for tourist information centers, historic districts, schools, etc. that meet the criteria for signing traffic generators shown in the TMUTCD when space is available.
- ◆ Cities may erect such signs on conventional highways and frontage roads of freeways, provided they are agreed to by TxDOT and they do not conflict with or obstruct existing signing. (This is presently covered in municipal maintenance agreements.)
- ◆ Signs placed by the city must not be attached to existing TxDOT supports.

#### **Commercial Names**

Traffic generators that meet the guidelines and have proper nouns such as "Ford Park" often hint at commercial signing. In this case the Ford Motor Company purchased the rights for the name of the facility, an entertainment center with various attractions. In these cases, TxDOT signs for the official name, regardless of its apparent commercial implication. On the other hand, a message such as "Sponsored by Ford Motor Company" would be deemed as advertising and would not be permitted under the standard set forth in the TMUTCD, Chapter 1A: "Traffic control devices or their supports shall not bear any advertising message or any other message that is not related to traffic control."

## Seasonal and Special Events Signing

Temporary signing on various routes to seasonal or special events attracting a large number of motorists should be provided similar to that for permanent generators. All temporary special-events signing must meet the requirements of Title 43 of the Texas Administrative Code (TAC), [Section 22.15](#) and be approved by the Maintenance Division.

## Airports

In 1988 there was considerable interest in trailblazing routes to international airports serving metropolitan areas from central business districts and convention center locations. The result of this effort was the guidelines for such ground-mounted signs (I-5 series) described in the [TMUTCD](#) and overhead signs described in the [Traffic Control Standard Plan Sheets](#). Cities were asked to add signs on their street system to enhance the state route signs.

In cases where there is more than one international airport, a name plaque may be added above the airplane symbol sign to define which airport was being signed.

All of these signs should continue to be maintained.

## Offices of Elected US and State Officials

Districts may erect modified D1 series guide signs with recommended 6-inch on conventional highways to indicate the local offices of U.S. and state representatives and senators in accordance with the criteria for state and federal government facilities outlined in the TMUTCD. The name of the elected official should be used on these signs. The representative or senatorial district number may also be included on the sign, but should not replace the name of the official. Phone numbers and physical or Web addresses are not allowed on these signs. The font size may be reduced if space limitations restrict the overall size of the sign.

The signs should be erected only when requested by the elected official. The distance restrictions in the signing criteria should be strictly followed. The signs may be erected on **conventional highways only**. Freeway and expressway main lane signing should be limited to the types of traffic generators listed for freeways. (Freeway frontage roads are considered conventional highways.)

## Texas Lottery Claim Centers

Districts should erect D1 guide signs for Texas Lottery claim centers on conventional highways in accordance with the criteria for state and federal government facilities outlined in the TMUTCD. When a lottery claim center is located in an existing comptroller's office and there is an existing

sign on the state system for COMPTROLLER OF PUBLIC ACCOUNTS, the lottery claim center sign should be mounted below it.

If the center is off-system and on a city street, districts should work with the city to have the sign erected at city or comptroller's expense.

## Section 10

### State Parks

#### Introduction

TxDOT provides an extensive system of guide signs (D7 series) for state parks.

See *Standard Highway Sign Design for Texas* [[link](#)] and the *Texas Manual on Uniform Traffic Control Devices* ([TMUTCD](#)) for sign details. Brown background color is used for recreational, cultural interest, and historical areas signing.

#### Sign Placement

Districts should make sure signing is compatible with the information shown on travel maps issued by TxDOT. On these travel maps, each park is listed and a “near city” is shown. Therefore, as a minimum, each park should be signed from the “near city” without regard to distance, number of routes, or annual attendance. This signing should originate on the closest major route (or routes) in the “near cities.”

Because of park attendance or other factors, signing for a park may extend beyond the “near city.” In addition, if a more direct route to the park is available from a location other than the “near city,” signing from this location is also encouraged.

## Section 11

### Public Beaches

#### Introduction

Senate Concurrent Resolution 46 of the 66th Texas Legislative, Regular Session, requested TxDOT to sign for free beach accessways and free beach-user parking areas. Minute Order No. 76653 dated January 23, 1980 directed the Engineer-Director to sign for warranted locations.

This section provides guidelines to administer this policy. Standard designs for the public beach access and parking signs (D55 and D56 series) are included in *Standard Highway Sign Designs for Texas* [\[link\]](#). Brown background color is used for recreational, cultural interest, and historical areas signing.

#### Access Roads

Public beach access roads should be signed with an advance sign 0.25 to 0.5 miles in advance of the intersection and a directional sign at the intersection.

A series of closely spaced access roads may have a combined advance sign with the text “NEXT XX MILES.” In such a case, the access roads should be numbered sequentially, and the number shown on each directional sign.

#### Parking Lots

When the public beach parking lot entrance fronts directly on a highway route, one advance sign should be erected 700 to 1500 feet in advance of the entrance, with a directional sign at the entrance. Multiple lots in the same vicinity with several access points on the same highway route should be numbered sequentially, and the number should be indicated on the appropriate signs.

#### Funding of Signs

Funding for public beach access and parking signs comes from the appropriate district account.

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## Section 12

### Wildlife Viewing Areas

#### Introduction

The “Watchable Wildlife Project,” coordinated by the Texas Parks and Wildlife Department (TPWD), has identified certain sites throughout the state as accessible wildlife viewing sites. The wildlife sites are described in the *Texas Wildlife Viewing Guide*, published by TPWD.

To promote environmental and ecological awareness and to help the public find the wildlife sites, TxDOT installs and maintains WILDLIFE VIEWING AREA signing (D7-12 and D7-13). See *Standard Highway Sign Designs for Texas* for [sign details](#).

#### Signing Standards

The WILDLIFE VIEWING AREA signing should correspond to the directions given in the *Texas Wildlife Viewing Guide*.

The 48 × 30 inch WILDLIFE VIEWING AREA sign should be placed at the nearest major intersection, and the 18 × 18 inch binocular-logo sign with supplemental arrow plaque should be used as necessary.

For sites on TxDOT right-of-way property, the actual site location may be signed with the binocular logo. Signing should **not** be used in locations where stopping to view wildlife could be hazardous. Signing required at off-right-of-way locations is financed and installed by others.

Many of the locations named in the *Texas Wildlife Viewing Guide* are state parks or other identifiable areas that may already be signed. In these cases, no additional signs are necessary. The existing guide signing for these areas, together with the guide directions, should normally suffice for guiding the motorist to these viewing areas.

Examples of a signed location and a complete list of Watchable Wildlife sites by district may be obtained from the Traffic Operations Division.

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## Section 13

### General-Services Signs

#### Introduction

The *Texas Manual on Uniform Traffic Control Devices* ([TMUTCD](#)) contains guidelines for businesses qualifying for general-services signing, both on freeways and conventional highways. This section expands and elaborates on the use of these signs.

#### Expanded Use on Conventional Highways

As a result of the emphasis on removing illegal advertising signs adjacent to highways, TxDOT has received complaints from businesses that without these advertising signs, motorists are unaware of their services. Often these businesses provide necessary motorist services (GAS, FOOD, LODGING, and CAMPING) for which general-services signs (D9 series) can be provided by TxDOT. General-services signs for qualifying businesses may help alleviate business owners' complaints of reduced retail sales due to the removal of illegal advertising signs.

In the past general-services signing (E20 series) was primarily used on freeways and expressways where an exit maneuver had to be made before the services became visible to motorists. General-services signing is usually unnecessary on conventional highways where the businesses are adjacent to and directly accessible from the highway. However, some businesses providing motorist services which are not directly visible or accessible from a conventional highway may be signed (D9 series) as referenced in the TMUTCD, Section 2E . 51. Districts should consider greater use of general-services signs in areas where motorist services are infrequent.

#### Verification

When requests are received for additional signs for motorist services (either on conventional highways or expressways and freeways), districts should verify that the service requesting signing meets the guideline criteria contained in the TMUTCD.

**For campgrounds**, the verification of acceptable drinking water supplies can be accomplished by contacting the regional engineer of the Texas Department of Health.

**For hospitals**, determination of the emergency medical facility's "level" may be found by contacting TRF, which has a list of qualifying hospitals supplied by the Texas Department of Health.

## Section 14

### Radio Information Signs

#### Background

The Federal Communications Commission (FCC) reserves certain AM broadcast frequencies for “travelers information stations.” The stations normally operate over a 2- to 10-mile radius and may broadcast information on:

- ◆ traffic and travel advisories
- ◆ availability of lodging, rest stops, and service stations
- ◆ descriptions of points of interest
- ◆ highway construction information
- ◆ on-site disaster information
- ◆ local emergency notifications
- ◆ weather (rebroadcasts of NOAA Weather Radio information).

Specifically **not** allowed on the broadcasts are:

- ◆ identification of commercial names
- ◆ commercial messages.

Districts may contact the Traffic Operations Division (TRF) for information on obtaining a license to operate a travelers information stations. The stations are also discussed in the *Radio Operations Volume* of the *Traffic Operations Manual*.

#### Signing

TxDOT installs and maintains signs identifying travelers information radio stations (D12-1 and D12-3) with the lead messages shown in the following table.

**Signing for Travelers Information Radio**

Lead Message	Note
TOURIST INFO	Tourist Information radio stations are usually purchased and operated by a local chamber of commerce, convention center, or tourist bureau.
WEATHER INFO	Weather Information Radio has had only limited use in far west Texas. The TMUTCD provides signing criteria for these stations.
HIGHWAY ADVISORY	Highway Advisory Radio (HAR) may be used on selected construction projects to enhance traffic operations.

## Section 15

### Reference Markers and Mileposts

#### Reference Markers

The Highway Reference Marker Location System has replaced the Milepost Marker System on all highways **except** the interstates. For installation and numbering system requirements for reference markers, see the [Texas Reference Marker System User's Manual](#). For specifications for reference markers (D10 series), see the [Traffic Control Standard Plan Sheets](#).

Reference markers should be accurately maintained so that reliable data can be gathered.

#### Milepost Markers

The Milepost Marker System remains in use on interstate highways (using the D10 series), because it is the national standard for the Interstate Highway System. Milepost markers, like reference markers, should be accurately maintained. The [TMUTCD](#) describes the applicable standards.

## Section 16

### Memorial Marker and Named Marker Highways and Structures

#### Introduction

A city, county, or the Texas Legislature may assign a memorial or other identifying designation to part of the state highway system (including a highway, bridge, or other structure). In referring to highways and structures so designated, TxDOT uses the modifiers “memorial marker” and “named marker.”

**Highways.** Normally identifying signs or special markers for memorial marker and named marker highways are not placed so as to be read from the highway main lanes, except in special cases specified by the Texas Legislature. They are typically placed on either end of the highway and at 75-mile intervals. They are usually placed in rest or picnic areas or turnouts. They are usually small and designed to be read by people on foot. The names never appear on intersecting cross streets or highways (this is covered in [“Street-Named Highways”](#)). For more information on signing standards, see [“Policy Guidance for Signing,”](#) [“Sign Placement Standards,”](#) and [“Sign Design Standards,”](#) later in this section. For exceptions see [“Exceptions to Signing Standards,”](#) later in this section.

**Bridges and Other Structures.** Memorial marker and named marker bridges and other structures are usually single bridges, interchanges, or even pedestrian bridges that have been given a memorialization or name by a city, county, or the Legislature. In most cases, the identifying instrument is a small marker or sign placed off the main lanes of the associated highway, typically in a parking or pedestrian area for reading by people on foot. For more information on signing standards, see [“Policy Guidance for Signing,”](#) [“Sign Placement Standards,”](#) and [“Sign Design Standards,”](#) later in this section. For exceptions see [“Exceptions to Signing Standards,”](#) later in this section.

#### Legal Basis for Naming and Signing

Texas Transportation Code, [Chapter 225](#), Subchapter A, grants local governments the authority to confer memorial or other names on state highways and portions of the state highway system and establishes how this is coordinated with TxDOT. TxDOT is specifically prohibited from naming or otherwise designating a highway, street, or bridge with any name or symbol other than the “regular highway number.” Subchapter B covers specific memorial marker and named marker highways established by the Legislature.

Title 43, Texas Administrative Code, [Section 25.9](#), provides the rules for implementing Chapter 225 of the Texas Transportation Code.

## Records Management

Memorial marker and named marker highways, bridges, interchanges, and other structures and historic routes are recorded and tracked by the Traffic Operations Division (TRF).

**Overlaps.** TRF strives to prevent overlaps of memorial marker and named marker routes. Overlapping is defined as two names covering the same highway route or corridor. Memorial marker and named marker bridges, structures, or interchanges on a memorial marker or named marker route or corridor do not constitute an overlap.

## Designation Process Varies

The process for designating and signing memorial marker and named marker highways and structures varies depending on whether the action is accomplished through local action (by a county or city) or through Legislative action. Descriptions of both processes follow under separate subheadings.

### Designation through Local Action

The following table describes the process by which a county, city, or public group designates a memorial marker or named marker highway, bridge, or other structure on the state highway system.

NOTE: The “requestor” mentioned in this process, which may be any interested party, must work through a city or county as the official sponsor.

#### Designation of Memorial or Named Highway or Structure through Local Action

Step	Responsible Party	Action
1	Requestor	Submits initial general request to the appropriate TxDOT district office. This general request is simply the basic information concerning the highway, bridge, or other structure and the proposed name.
2	District office	Checks with TRF to verify that the highway or structure does not already have a name. If the facility is not already named and the requestor is still interested, then the district advises the requestor to work through the county or city. The district should provide the requestor with general information concerning the process, including the requirements and standards contained in this section.
3	City or county	Passes the necessary resolution or ordinance and furnishes it to the district along with the details of the marker, plaque, or sign (size, proposed wording, color, and any other pertinent information).
4	District office	Investigates to see if satisfactory space is available for the markers, plaques, or signs to be located safely and without interference with maintenance activities. For further details on signing standards and exceptions, see <a href="#">“Policy Guidance for Signing,”</a> <a href="#">“Sign Placement Standards,”</a> and <a href="#">“Sign Blue Standards,”</a> later in this section.

**Designation of Memorial or Named Highway or Structure through Local Action**

Step	Responsible Party	Action
5	District office	Forwards the information to TRF for final approval of the size, color, and wording.
6	TRF	Reviews the request, and if the request is acceptable, records the name and limits (if a highway is involved) and prepares an approval memo for the executive director's signature. See " <a href="#">Approval of Names</a> ," later in this section.
7	TxDOT executive director	Signs the approval memo, which is then forwarded to the district office.
8	District office	Negotiates an advanced funding and general terms agreement with the sponsor. (The sponsor gets one original and the district retains one original.) See "Financing and Agreement" following this table.
9	District office	Installs the markers, plaques, or signs after the advanced funding arrives and as time and conditions permit.

**Financing and Agreement.** The requestor must pay for the sign, marker, or plaque and its installation through the sponsor. The sponsor (county or city) must sign an advance funding and general terms agreement with the state. The relevant agreement — "Named Marker or Memorial Marker Highway Sign Agreement" — can be accessed through the TxDOT intranet (accessible only within the TxDOT network) at <[http://crossroads.org/cso/standard\\_contract\\_forms.htm#Traffic](http://crossroads.org/cso/standard_contract_forms.htm#Traffic)> under "Traffic."

**Replacement of Signs and Maintenance of Grounds.** If the marker, plaque, or sign is damaged or needs replacing, the sponsor must provide a new or refurbished marker, plaque, or sign upon notification by TxDOT. Failure to do so within a reasonable time (usually 6 months) will result in the removal of the marker, plaque, or sign in accordance with the terms of the agreement. The agreement can be revised if the length of time is not reasonable due to the frequency of the sponsor's official meeting schedules. TxDOT maintains the grounds where the marker, plaque, or sign is displayed.

**Designation through Legislative Action**

The Texas Legislature may designate a memorial marker or named marker highway, bridge, or other structure on the state highway system.

After legislation is passed, TRF notifies the district or districts involved and develops a legislative implementation plan. The district or districts work with TRF to erect the signs.

**Financing.** Unless the enabling legislation provides for a grant or donation through a private entity to assist in financing the construction and maintenance of the sign or signs, TxDOT bears all expenses associated with the signing.

If a grant or donation will be provided, then the process follows the “Instructions for Acceptance of Donations” outlined in [Chapter 10](#) of the *Contract Management Manual*. After the private entity provides the funding, TxDOT lets a contract or uses state forces to install the markers (signs). The district should not accept any checks until the Commission approves the donation and the Donation Agreement (if required) is executed.

## Approval of Names

TxDOT has the authority to review the text and approve or disapprove the marker, plaque, or sign message for memorial and name designations proposed by cities and counties.

**Use of Commercial Names.** If the text is determined to border on commercial advertising (such as “Ford Motor Company Freeway”) it would be turned down because the *Texas Manual on Uniform Traffic Control Devices* (TMUTCD) forbids commercial advertising. However, if the designation were “Henry Ford Freeway,” for example, then TxDOT would approve it. The name of a person, whether dead or alive, is acceptable. Note that the policy against commercial names for memorial marker and named marker highways is more restrictive than that applied to signing for traffic generators (see [“Commercial Names”](#) in Section 9 of this chapter).

**Name Must be Inoffensive.** All designations must be reasonably inoffensive to the public as determined by TxDOT.

## Policy Guidance for Signing

The TMUTCD contains the following guidance ([Section 2E.08](#)):

Freeways and expressways should not be signed as memorial highways, except in special cases specified by the Texas Legislature. If a route, bridge, or highway component is officially designated as a memorial, and if notification of the memorial is to be made on the highway right of way, such notification should consist of installing a memorial plaque in a rest area, scenic overlook, recreational area, or other appropriate location where parking is provided with the signing inconspicuously located relative to vehicle operations along the highway, unless otherwise provided for by state statute.

As an option, the TMUTCD suggests the following:

If the installation of a memorial plaque off the main roadway is not practical, a memorial sign may be installed on the mainline.

The TMUTCD lists the following standards where memorial signs are installed on the mainline:

- ◆ Memorial names must not appear on directional guide signs.
- ◆ Memorial signs must not interfere with the placement of any other necessary highway signing.
- ◆ Memorial signs must not compromise the safety or efficiency of traffic flow.

The TMUTCD further requires that memorial signing be limited to one sign at each end of the designated limits, and at such intermediate sites that markers are approximately 75 miles apart.

The TMUTCD offers similar guidance for conventional roads and streets in [Section 2D.48](#).

## Sign Placement Standards

**Frequency of Markers for Highways.** For memorial marker and named marker highways, Texas statutes allow for intermittent markers every 75 miles and one marker on each end of the designated highway.

### Examples:

- ◆ **Example 1:** A designated highway is 140 miles long, so it gets one marker at each end and one in each direction at the 75 mile point from each end approximately.
- ◆ **Example 2:** A designated highway is 100 miles long, so it is eligible for a marker on each end and the 50 mile points for each direction. This highway technically would have the intermittent markers at 75 miles, but it makes more sense in this case to place the markers midway as long as the entire length is more than 75 miles. However, the literal interpretation can also be applied if necessary and if the sites are available.

The main-lane signs, when used, should be located at a safe and non-obtrusive site just after the beginning of the designated highway, typically within  $\frac{1}{4}$  to  $\frac{3}{4}$  mile if practical. Markers in rest areas, scenic areas, turn-outs, etc. may be located wherever practical. Sometimes only one marker will be erected, depending on the sponsor's request. Signs should not be posted on the main lanes, unless it is impractical to do otherwise. The exception is for routes designated by the Legislature. In this case a precedent has been set to install signs on the main lanes always and to follow other specific instructions contained in the statute.

**If More Than One Numbered Highway Is Involved.** Occasionally a memorial marker or named marker highway follows the path of more than one numbered highway. In such cases, each differently numbered segment of the named highway will receive a marker at each end and, if a segment is more than 75 miles in length, intermittent markers every 75 miles.

**For Bridges and Other Structures.** For memorial marker and named marker bridges and other structures the marker should be installed out of sight of the main lanes, if practical. Generally only one plaque or marker is furnished by local authorities for installation in a viewing area. Sometimes one sign on each approach is required if the legislature is involved, or if there are no practical viewing areas, then a sign is required on the main lanes.

## Sign Design Standards

**For Non-Highway Viewing.** Memorial marker and named marker highway and structure signs not facing the main lanes (those mounted in rest areas, picnic areas, turnouts, parking areas, etc.) should be simple in design and relatively small. If a sign is used, it should have a green background with white legend. If a plaque or marker is used, it should have a black or green background with a white, aluminum, or silver legend. Some variation may be allowed as long as the design is in good taste. None of these signs or markers (plaques) located off the main lanes need to be reflective. The legend size must be for pedestrian reading, not for high speed traffic viewing, generally one-inch letters or smaller. They should be limited in size to what will fit on one pipe support. Signs with breakaway features should be used if subject to impact by vehicles traveling more than 10 mph. Plaques and markers need to be protected or placed well out of the [clear zone](#), since these are not breakaway tested.

**For Highway Viewing.** Memorial marker and named marker highway and structure signs mounted for viewing from highway lanes fall under the category of general information signs. They must be reflective with green background and white legend. The lettering standard, varies according to the highway type, as follows:

### Signs Mounted for Viewing from Highway Lanes

(Memorial Marker and Named Marker Highways and Structures)	
Highway Type	Typical Lettering Standard
Freeways (including Expressways)	Clearview font with uppercase sized at 13.3 in. (Clearview font automatically sizes the lowercase at 10 in.) NOTE: This is essentially the same size as the place name for supplemental freeway signs in the current TMUTCD; however, the TMUTCD does not mention expressways or the lowercase sizes.
Conventional Highways	Clearview font with uppercase of 8 in., lowercase 6 in.

## Exceptions to Signing Standards

**Signs for Designated Highways.** For memorial marker and named marker highways, exceptions to the above sign placement standards are made in accordance with legislative requirements or where there are no other suitable alternatives. Occasionally legislation has called for signs on the main lanes and at specific spacings. In these cases the highway number and memorial or name designation are both on the same sign and on the main lanes.

**Signs for Bridges and Other Structures.** For memorial marker and named marker bridges and other structures, exceptions to the above sign placement standards are made in accordance with legislative requirements or where there are no other suitable alternatives. For example, in special cases involving a legislative request or lack of a practical viewing area, a general information sign carrying the name of the structure is installed on the main lanes.

## Section 17

### Historic Routes

#### Introduction

A county historical commission working through the Texas Historical Commission may assign an historic route designation to a farm-to-market or ranch-to-market highway. Signing for such routes is placed at the beginning and end of the highway (TxDOT practice allows a maximum of two signs per route). Signs are reflective and have a brown background and white legend. For further details on signing standards, see [“Sign Placement and Standards”](#) later in this section.

#### Legal Basis for Naming and Signing

Texas Transportation Code, [Chapter 225](#), Subchapter A, establishes the process for designating historical routes through the Texas Historical Commission in coordination with TxDOT.

Title 43, Texas Administrative Code, [Section 25.9](#), provides the rules for implementing Chapter 225 of the Texas Transportation Code.

NOTE: TxDOT is specifically prohibited from naming or otherwise designating a highway, street, or bridge with any name or symbol other than the “regular highway number.”

#### Records Management

Historic routes are recorded and tracked by the Traffic Operations Division (TRF).

#### Designation Process

The process by which a farm-to-market or ranch-to-market road may be designated as a historic route is as follows:

##### Designation of Historic Routes

Step	Responsible Party	Action
1	County historical commission	Applies to the Texas Historical Commission and TxDOT for marking an FM or RM road with an historic name. The application should include evidence showing that the name has been in common usage in the area for at least 50 years, and must be supported by affidavits from at least five long-time residents of the area.
2	Texas Historical Commission	Certifies, based on the evidence submitted by the county historical commission, that the name has been in common use in the area for at least 50 years
3	Texas Historical Commission	Submits a request to TxDOT along with the certification and supporting documents and the name of the historic route.

### Designation of Historic Routes

Step	Responsible Party	Action
4	TRF	Reviews the certification and sends it to the appropriate district office.
5	District office	Contacts the county historical commission that originally applied for the designation and initiates an advanced funding and general terms agreement, which requires that the county historical commission pay the cost of preparing, installing, and maintaining the signs.
6	County historical commission	Executes the agreement and provides the necessary funding to the state. See <a href="#">“Agreement”</a> following this table.
7	District office	Prepares and installs the signs indicating the road’s historic name (see <a href="#">“Sign Placement and Standards”</a> later in this section), then informs TRF and the county historical commission that the signs have been installed.

### Agreement

Before TxDOT installs an historic route sign on the right of way, an executed “County Historical Commission Historical Route Sign Agreement” is required. This agreement can be accessed through the TxDOT intranet (accessible only within the TxDOT network) at [http://crossroads.org/cso/standard\\_contract\\_forms.htm#Traffic](http://crossroads.org/cso/standard_contract_forms.htm#Traffic) under “Traffic.”

### Sign Placement and Standards

TxDOT fabricates and erects a maximum of two signs per historic FM or RM route.

**Sign Placement.** Historic route signs must be mounted independently in accordance with TxDOT installation procedures and at a suitable location near each end of the route. The signs **must not** be erected within 1,000 feet of an intersection. The signs **must not** be mounted on the same support with other traffic control signs. See Figure 7-7.

NOTE: The highway route markers must **not** contain the historic route name.

**Sign Standards.** Historic route signs must be reflective and fabricated in accordance with TxDOT specifications and procedures. The sign must have a brown background with a white border and white, 8- or 6-inch Clearview letters (8-inch preferred). Overall dimensions may vary; however, it is recommended the text be limited to 3 lines. Figure 7-6 shows a sample sign layout, and Figure 7-7 shows a suggested signing schematic.



Figure 7-6. Typical historic route sign. Eight-inch letters are preferred, but 6-inch letters are acceptable.

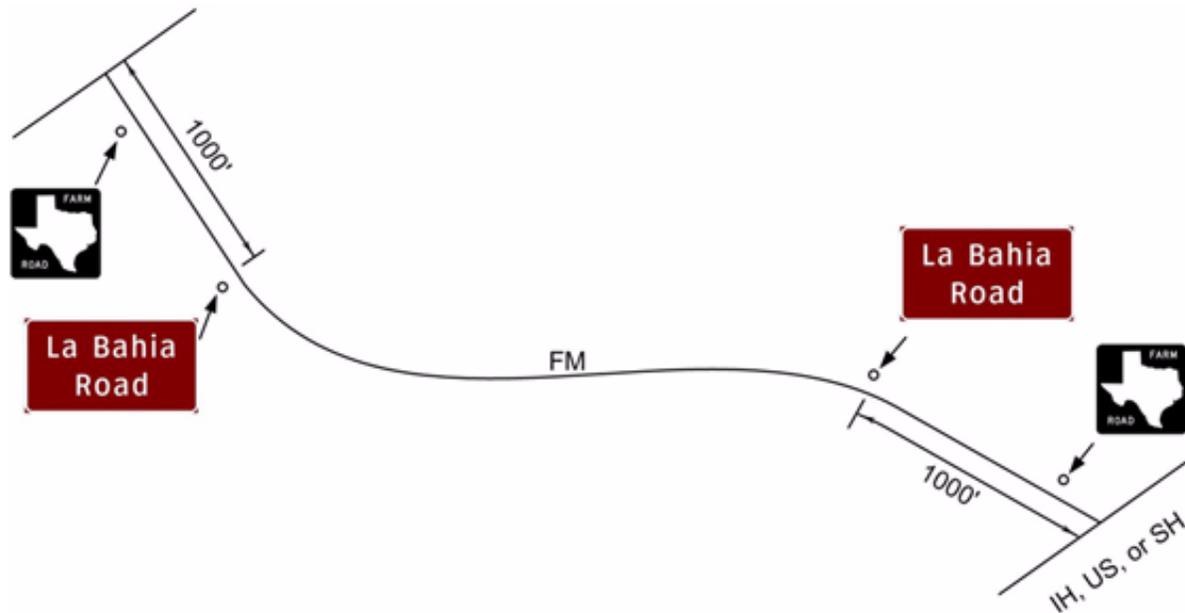


Figure 7-7. Layout and suggested schematic for historic route signs.

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## Section 18

### Street-Named Highways

#### Introduction

Street-named highways bear names that only appear on the cross streets or intersecting highways. These commonly appear as street signs mounted at intersection corners or on traffic signal mast arms. However, when a freeway or expressway crosses a street-named highway, the name may appear on guide signs directing motorist to the street-named highway. These street names never appear on or visible to the street-named highway itself.

#### Legal Basis for Naming

Texas Transportation Code, [Chapter 311](#) contains “General Provisions Relating to Municipal Streets.” Sections 311.001–003 grant municipalities control over their streets. This control extends to assigning street names to highways within their jurisdiction.

NOTE: Counties are not allowed to “street name” numbered highways except in unusual circumstances where mail delivery is a factor.

NOTE: TxDOT is specifically prohibited from naming or otherwise designating a highway, street, or bridge with any name or symbol other than the “regular highway number.”

#### Records Management

Street-named highways are not tracked by TRF. Each district should keep track of street-named highways within its boundaries.

#### Designation Process Varies

The process for designating and signing a street-named highway differs depending on whether it is done by a city or the Legislature. Descriptions of both processes follow under separate subheadings.

#### Designation by a City

A city may assign a street name to a portion of highway within its limits. Counties may not assign street names to numbered highways, except in unusual circumstances where mail delivery is a factor.

The process by which a city normally designates a portion of highway within its limits with a street name is as follows:

#### Designation of Street-Named Highways

Step	Responsible Party	Action
1	City	Enacts a resolution or ordinance naming the highway with a street name and sends the TxDOT district office a letter of notification and a copy of the official document. If more than one jurisdiction is involved, then each jurisdiction must provide a notification letter and copy of the official document.
2	District office	Sends the document to TRF-TE for review.
3	TRF	Reviews the name for possible problems.
4	District office	May change out guide signs on crossing freeways, expressways, etc. in accordance with payment conditions outlined following this table and after the execution of any necessary agreement. For details on signing see <a href="#">“Signing Practices”</a> later in this section.

**Payment for Street-Name Signs.** Street-name signs on corners and frontage roads are the responsibility of the city. Street name signs on special advanced overhead-pole structures and traffic signal poles are paid for by TxDOT if the traffic signal or advanced pole structure belongs to TxDOT, unless the existing Municipal Maintenance Agreement (MMA) states otherwise.

**Payment for Guide Signing.** If the street name change requires the replacement of existing guide signs or structures, TxDOT will ask the city to pay for the new signs and structures when both of the following are true:

- ◆ The total expected cost of the change is greater than \$500 **and**
- ◆ the existing affected signs and structures are not scheduled for replacement within two years.

**Agreement.** Generally if the cost of the changes will be more than \$500, an advance funding agreement is pursued (contact the Office of General Counsel (OGC), Contract Services Section for assistance). If an agreement will be pursued, first determine if a master advance finding agreement (MAFA) with the city exists by checking the [“MAFA Spreadsheet](#) (Local Governments currently in MAFA program)” at <http://crossroads.org/cso/MAFAs.xls>. Then proceed as follows:

If...	Then...
an MAFA exists	use the short-form agreement — “LPFA for Voluntary Local Government Contributions to Transportation Improvement Projects with No Required Match.”
an MAFA does not exist	use the long-form agreement — “AFA for Voluntary Local Government Contributions to Transportation Improvement Projects with no required match (No Federal/State Funding).”
Both agreement forms are available at <a href="http://crossroads.org/cso/standard_contract_forms.htm">http://crossroads.org/cso/standard_contract_forms.htm</a> (accessible only within the TxDOT network). Look under “Advance Funding Agreements” (AFA).	

### Designation by the Legislature

The Legislature may assign a street-name to a highway. After such legislation is passed, TRF notifies the district or districts involved.

**Financing.** Unless the enabling legislation provides financing, TxDOT bears all expenses associated with the signing.

### Signing Practices

Details for freeway and expressway guide signing for street-named highways are shown in Figure 7-8 through Figure 7-11.

**Crossing Street Names on Bridges.** On freeways and expressways in large urban areas, the crossing street name may appear on the crossing roadway bridge. This practice is allowed to assist motorists in using maps. These signs are usually a little smaller than the clearance signs on the bridges and located so they won't interfere with the clearance signs. These are not guide signs, but simply street signs mounted on bridges and paid for, installed, and maintained by TxDOT.

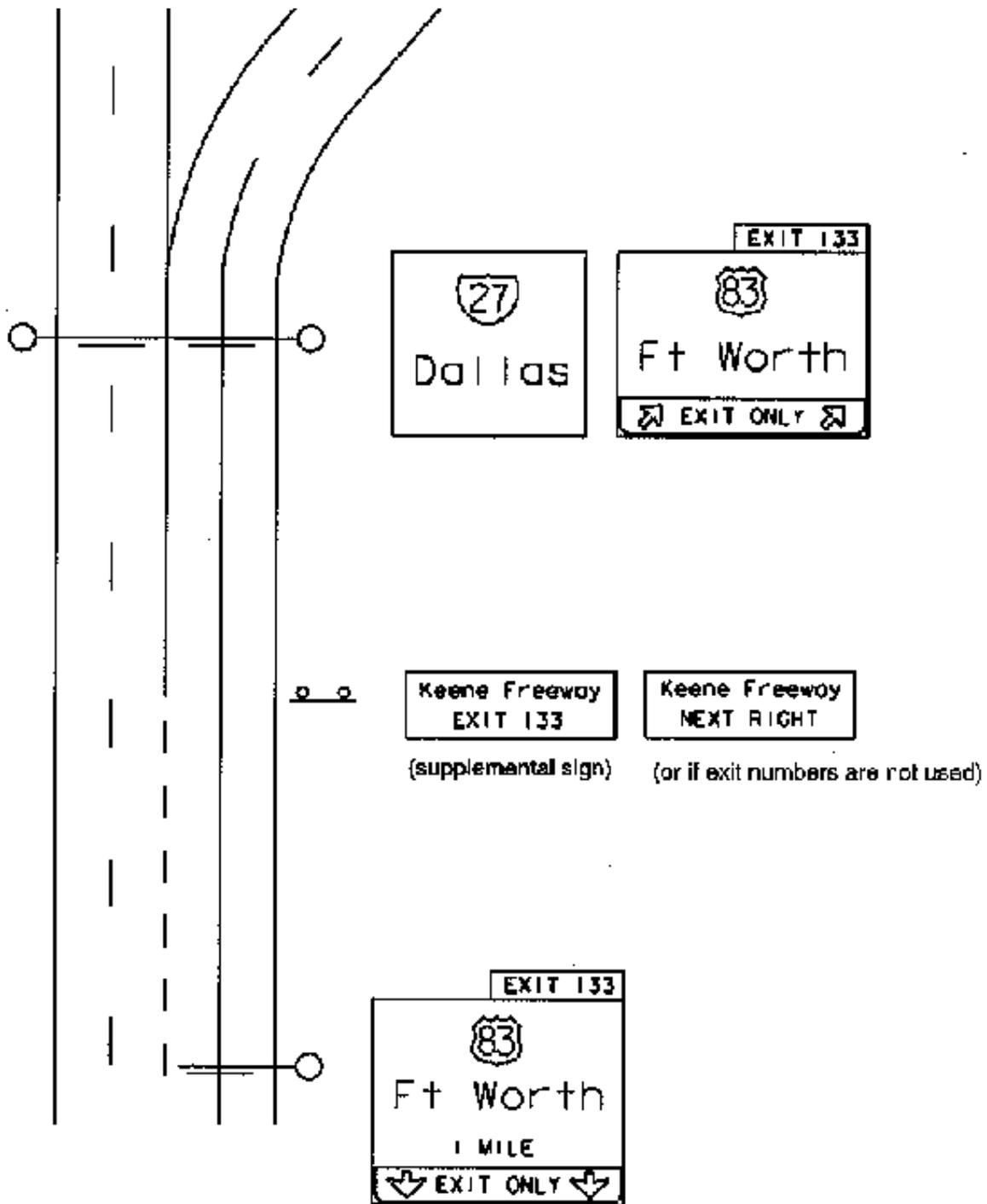


Figure 7-8. Signing on an expressway or freeway for a named intersected freeway route.  
**Note:** A supplemental sign may be used only when space is available. Freeway name should only be used when name applies to entire length in urban area, even when more than one jurisdiction is involved.

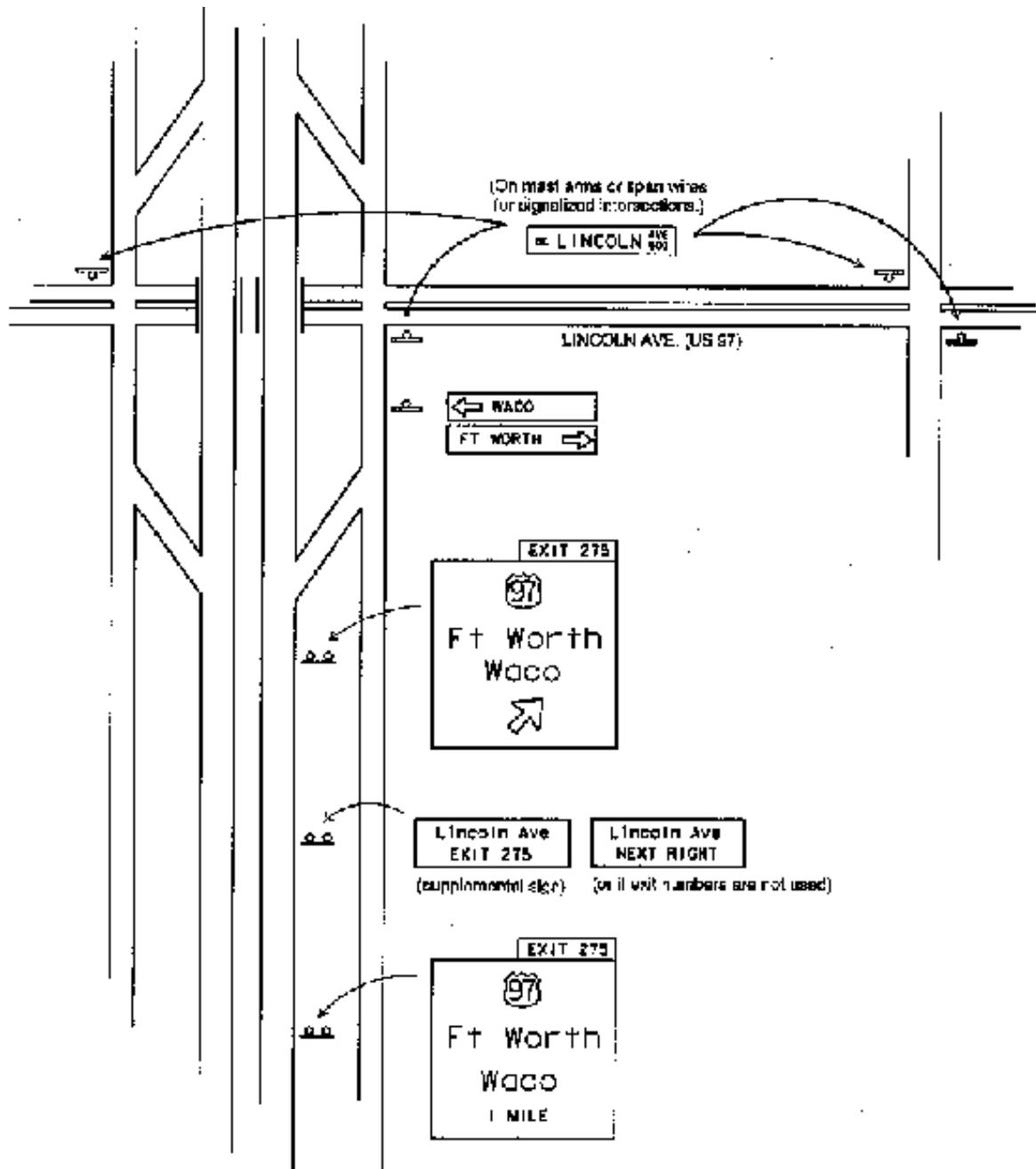


Figure 7-9. Normal case signing on an expressway or freeway for an intersected highway route that is a named city street. (For information on oversized street name signs at intersections, see [Section 3](#) of this chapter.)

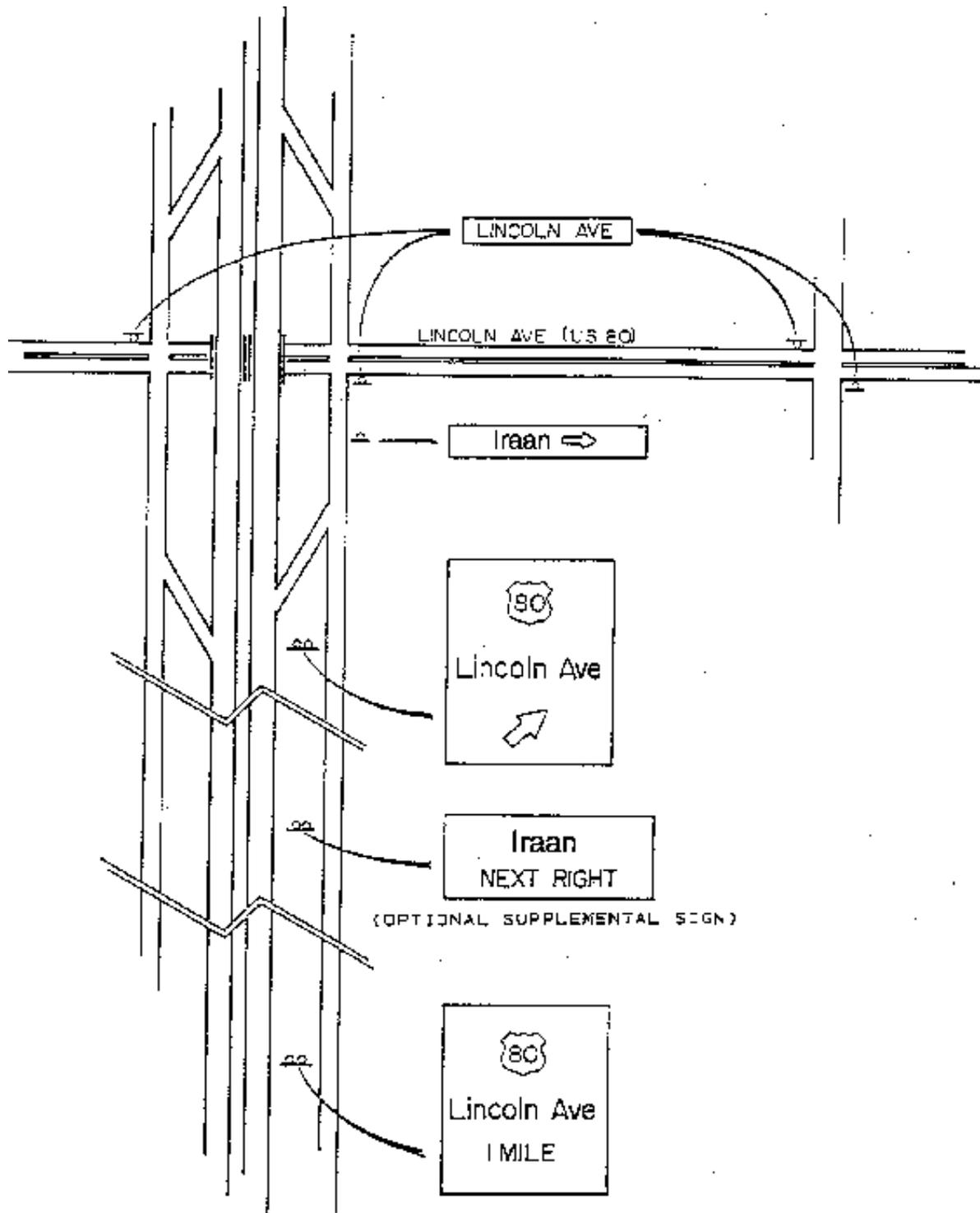


Figure 7-10. Special case signing on an expressway or freeway for an intersected highway route that is a named city street that is more significant than the destination name.

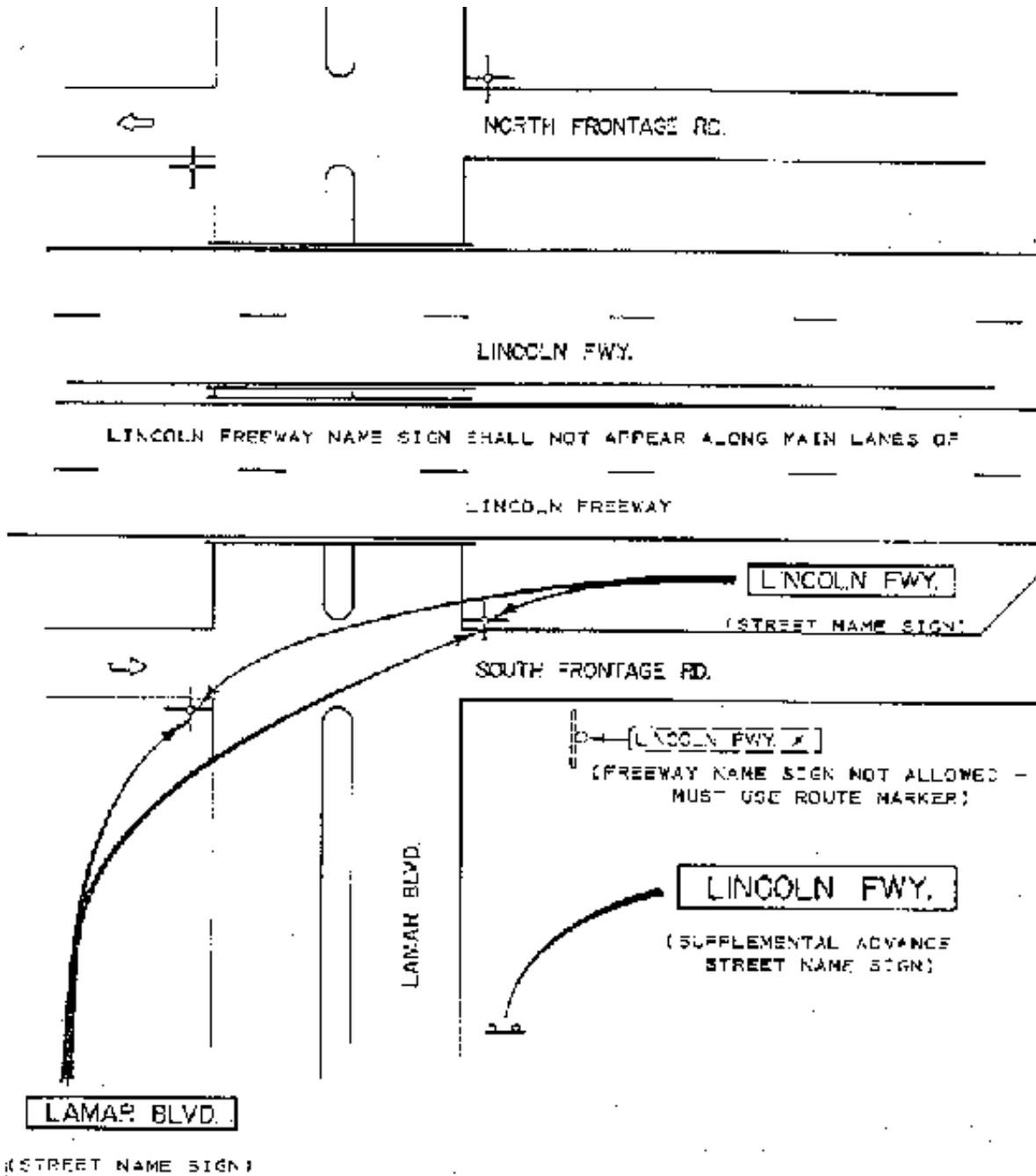


Figure 7-11. Street signs on city streets for a named freeway route.  
**Note:** Street name signs are usually the city's responsibility under standard maintenance agreement.

## Section 19

### Historical Markers

#### Introduction

Historical markers are used to interpret, promote, and protect historic and cultural resources. Requests for markers are initiated through local county historical commissions and approved through the Texas Historical Commission (THC). TRF works with the THC to approve markers placed on TxDOT right of way. TxDOT provides advance and directional guide signs for all historical markers on TxDOT right of way outside of city limits on conventional highways.

#### Historical Marker Process Overview

All requests for historical markers made by the public should be directed to the local county historical commission. If the marker will be placed on TxDOT right of way, the process of approval and erection proceeds as follows:

##### Historical Marker Process — Request, Approval, and Installation

Step	Responsible Party	Action
1	County historical commission	Upon concurring with the request, applies to the Texas Historical Commission (THC). The necessary forms and instructions can be downloaded at < <a href="http://www.thc.state.tx.us/markersdesigns/madmark.html">http://www.thc.state.tx.us/markersdesigns/madmark.html</a> >
2	THC	Reviews the request and, upon approval, sends a memorandum detailing the proposed historical marker and its wording, along with the marker number (from the <a href="#">Texas Historic Sites Atlas</a> ), job number, and contact person, to TRF.
3	TRF	Reviews the memorandum and associated data and, upon approval, sends a form memorandum, along with the marker and job number and Atlas number and name of contact person, to the appropriate district office.
4	THC	Provides the historical marker to the district.
5	District office	Works with the chairman of the local county historical commission to determine the actual placement location for the historical marker (see following segment on “Marker Placement”). The district installs the marker (see “Marker Design and Installation” later in this section) and also provides and installs advance and directional guide signs using the guidelines in this section and the <i>Texas Manual on Uniform Traffic Control Devices</i> (TMUTCD).

#### Marker Placement

The safety of the traveling public is a major priority in the location of the historical marker itself. The markers are not mounted on breakaway supports and have not been tested for vehicle impacts;

therefore, they are to be considered non-breakaway. They should be located outside the [clear zone](#) or protected or not installed at all.

TxDOT policy specifies that markers be located in roadside parks, at existing turnouts, or at a specially prepared turnout. The primary effort in locating these markers should be to utilize existing facilities. Where such cannot be done, locations should be selected that would require a minimum amount of improvement. In these instances, the usage by the public should determine the extent of development necessary. Often soil conditions will require only minor grading or a minimum amount of stabilization. Surfacing will be justified for very few marker facilities. TxDOT strives to cooperate with the historical marker program by placing markers in locations accessible to the public. Nevertheless, the scope of the program is such that the economic aspects deserve careful attention.

Marker placement can be discussed with the county historical commission.

### **Design and Installation of Markers**

All Texas historical markers must conform to the standard design approved by agreement between TxDOT and THC. Free-standing markers consist of a one-piece aluminum marker welded to a 7-foot aluminum pole. The pole should have a mark indicating the depth to which the pole needs to be buried within the concrete foundation.

New markers are shipped directly from the foundry to the district. TxDOT only furnishes the foundation and the labor to install the marker.

### **Maintenance of Markers**

The district maintains the grounds around the historical markers on TxDOT right of way.

If the historical marker is damaged or vandalized, THC is responsible for cleaning, repair, or providing a new replacement marker. The district should notify THC's chief historian of the need for maintenance (cleaning, etc.) or replacement. (THC's phone number is 512-463-5853.) THC then works directly with the local county historical commission, who is responsible for cleaning or replacing the marker. Replacement markers must conform to the approved standard design described under "[Design and Installation of Markers](#)" above.

### **Marker Numbers**

As part of a statewide identification and reference system, the THC has assigned marker numbers (also called "Atlas numbers" because they are compiled in the [Texas Historic Sites Atlas](#)) to all historical markers.

State law (Chapter 442, Texas Government Code, Section 442.0065) requires that all advance and directional historical-marker guide signs that are newly installed or replaced on a maintenance basis include the relevant marker's identifying number. This requirement affects D7-6, D7-7, D7-8, D7-7a R(L), and related signs (see "[Advance and Directional Signing Standards](#)" later in this section).

**Finding the Number.** The best way to find the marker number for a sign being replaced on a maintenance replacement basis and which currently does not display a marker number is to use the THC's on-line [Texas Historic Sites Atlas](#). The Atlas can provide a list of historical markers by county. Each marker's number is listed, along with other pertinent information such as text, site name, and address. To find a marker number, proceed as follows:

1. Go to <http://atlas.thc.state.tx.us>
2. Click on "**Enter the Atlas**" at the bottom of the page.
3. Under "Keyword and Phrase Search," set the following search criteria:
  - Under "Word(s) or Phrase(s)," enter "**Full Record.**"
  - Under "Scope of Search," select the "**County**" button and then select a county name from the pull-down list.
  - Under "Record types to view," uncheck everything except "**Historical Markers.**"
4. Click the **Search** button. The "full record" of historical markers for the county you selected appears in the left frame of the screen.
5. Find the marker you're looking for and click on "**Historical Marker.**" Information about the marker appears in the right frame of the screen with the marker number listed at the very top.
6. Verify the location by clicking on "**Location Map,**" if available.

NOTE: The web site lists **all** historical markers — off-system, on-system, and those within city limits.

### Booklet Available

Also, in compliance with the same law, the Travel Division (TRV) publishes a booklet, *Texas Historical Markers*, providing brief descriptions (including Atlas marker numbers) of those historical markers located on on-system conventional highways outside of cities. The booklet is available to the public for free, and will be updated as determined by TRV. TRF maintains a database for TxDOT use with these historical markers and their corresponding numbers that are or will eventually be signed for on conventional highways. This database is used to compile the booklet.

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## Advance and Directional Signing Standards

Advance and directional historical marker guide signs provide motorists guidance to historical marker sites on TxDOT's conventional highway system outside of city limits. Only historical markers within the right of way of an on-system conventional highway are eligible to be signed for by TxDOT.

All historical markers installed after November 1, 1962, should have advance guide and directional signing. As explained earlier, all new or replaced historical-marker guide signs must display the Atlas marker number (see following segment on [“Advance and Directional Sign Details”](#)).

**Placement.** Generally the advance guide sign is placed approximately one mile before the directional sign. If the roadway is one lane in each direction and undivided, then at the engineer's discretion, the directional signs at the marker may be mounted on one side of the road, back-to-back.

**Markers Within Cities.** Although TxDOT does not sign for individual historical markers within incorporated cities or those not on TxDOT right of way, the HISTORICAL MARKERS IN CITY signs, which are furnished by THC, are used to alert motorists that a city has historical markers within its city limits. This sign should be incorporated into the City Pride Sign Program (see Chapter 8, Section 9, for information).

If a city incorporates an existing historical marker into its city limits, which is on one of TxDOT's conventional highways, the directional and advance historical marker guide signs should be removed at the end of their useful life. TRF should be notified (including a brief description of the historical marker content along with the marker number) by memorandum or e-mail when these signs are removed. TRF will notify TRV so that TRV can update their booklet.

## Advance and Directional Sign Details

Details of various advance and directional historical marker guide signs with Atlas marker numbers are shown in Figure 7-12 through Figure 7-14.

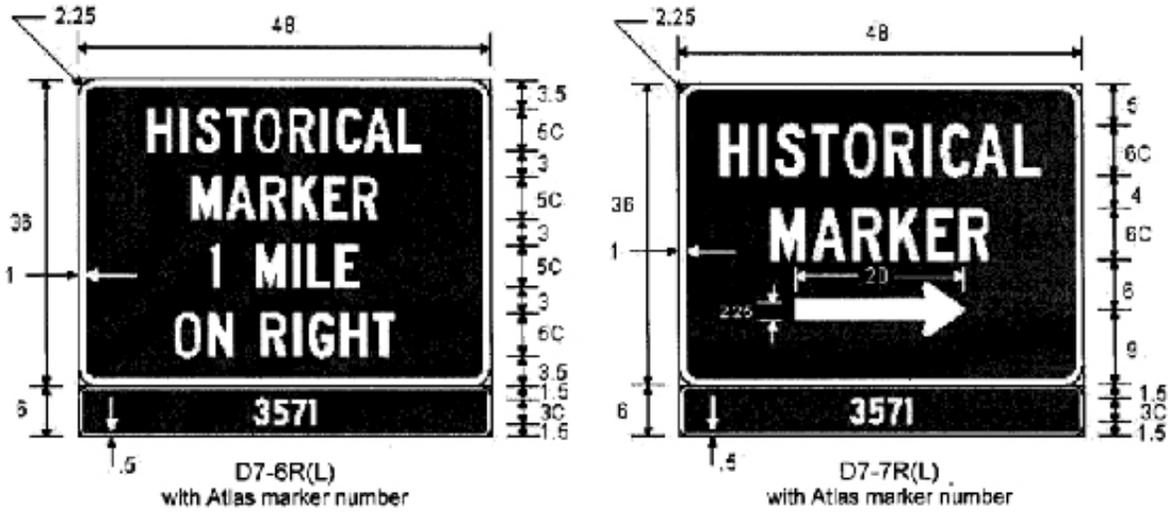


Figure 7-12. Advance and directional historical marker guide signs with Atlas marker numbers.  
 Colors: legend — white (retroreflective); background — brown (retroreflective)

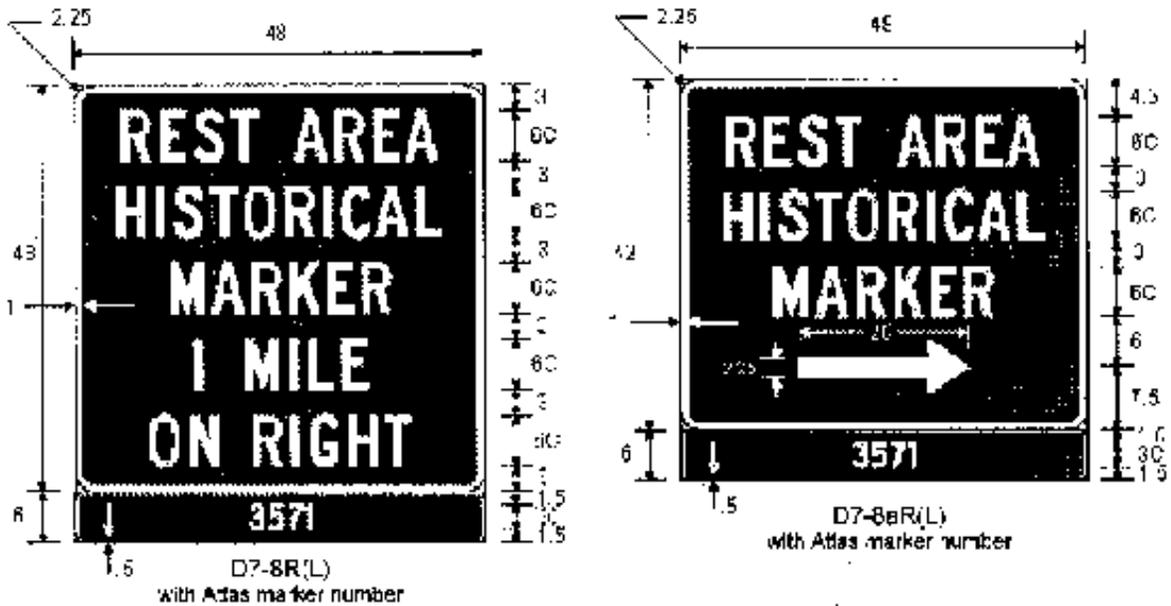


Figure 7-13. Advance and directional rest area historical marker guide signs with Atlas marker numbers.  
 Colors: legend — white (retroreflective); background — blue (retroreflective)

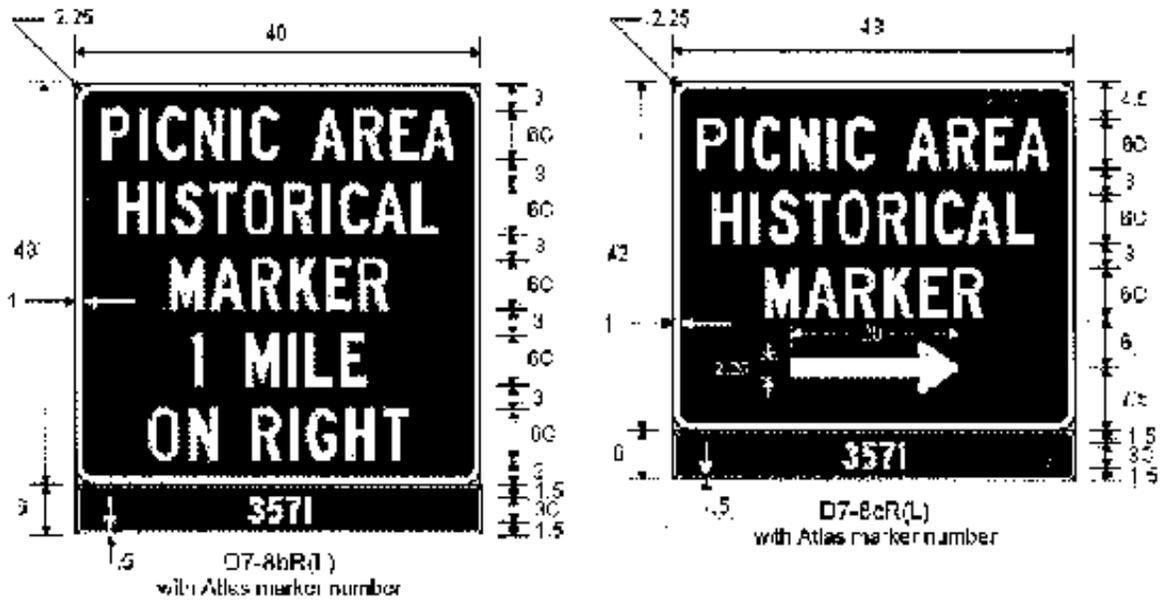


Figure 7-14. Advance and directional picnic area historical marker guide signs with Atlas marker numbers.

**Colors:** legend — white (retroreflective); background — blue (retroreflective)

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## Section 20

### County Line Signs

#### Expressways and Freeways

County line signing for expressways and freeways should conform to the [TMUTCD](#). An I-2d type sign design should be used, incorporating a recommended minimum 8-inch letters. [Dimensions](#) for the I-2d type sign can be found in *Standard Highway Sign Designs for Texas*.

#### Placement

On low-volume farm to market roads, county line signs may be mounted back-to-back. High-volume farm to market roads and U.S. highways should use independent mounts for county line signs in each direction.

## Section 21

### WELCOME TO TEXAS Signs

#### Introduction

TxDOT installs WELCOME TO TEXAS signs on each interstate, U.S., and state highway entering the state. Materials for these signs may be obtained from the General Services Division's Regional Distribution Centers.

#### Standards

Texas Transportation Code, Section 201.617 requires the following elements on the WELCOME TO TEXAS sign:

- ◆ a depiction of the state flag
- ◆ the phrase "Drive Friendly – the Texas Way"
- ◆ the phrase "Welcome to Texas – Proud Home of President George W. Bush."

shows the WELCOME TO TEXAS sign. The sign background is reflective interstate green. The legend and border are reflective white. The flag is red, white, and blue, as per the state flag. The "Proud Home" plaque may be a separate attachment.



Figure 7-15. WELCOME TO TEXAS sign for interstate highways.

**Sign Size.** The size of the sign and the accompanying plaque depends on the type of highway on which it is used, as shown in the following table

**Welcome to Texas Sign Sizes**

Highway Type	Parent Sign	Plaque
Interstate	18 × 11	18 × 1.5
U.S. and State Routes	12 × 8	12 × 1
FM Routes	6 × 4	6 × 0.5

### Placement

WELCOME TO TEXAS signs should be erected as close to the state line as practical.

**Interstate Routes.** On interstate routes, districts should make sure the welcome sign does not conflict with existing signing and submit schematics showing the proposed location to the Traffic Operations Division (TRF) for review.

**U.S. & State Highways.** On U.S. and state highways, the welcome sign should be located within 500 feet of the border.

## Section 22

### Accessibility Signing for Rest and Picnic Areas

#### Introduction

Many of the rest and picnic areas throughout the state have been modified to meet the requirements of the Americans with Disabilities Act (ADA). The signs described in this section are intended to better inform the public of TxDOT's continuing effort to provide accessibility at all facilities.

#### Use of the Accessibility Plaque

**Divided Highways and Freeways.** Rest and picnic areas on divided highways and freeways that have been modified to meet ADA requirements should be signed with the D9-6 accessibility plaque as illustrated in Figure 7-16. The D9-6 accessibility plaque should be attached to the parent sign, not to the parent sign supports (see Figure 7-16). The preferred position is above the parent sign, but if it is mounted below, a 7-foot minimum clearance between the bottom of the plaque and the ground must be maintained.



Figure 7-16. ADA accessibility plaques (D9-6) attached to advance picnic and rest area signs for divided highways and freeways.

**Conventional Roadways.** Rest and picnic areas on conventional roadways that have been modified to meet ADA requirements should be signed with the [D9-6a](#) accessibility plaque (Figure 7-17), indicating that the facility provides ADA accessibility to one or more picnic areas. The D9-6a accessibility plaque may be attached above or below existing advance and directional REST AREA and PICNIC AREA ([D5 series](#)) signs (as shown in Figure 7-17) or incorporated as a standard design for new signs (shown in *Standard Highway Sign Designs for Texas*). If the plaque is mounted below the parent sign, a 7-foot minimum clearance between the bottom of the plaque and the ground must be maintained.

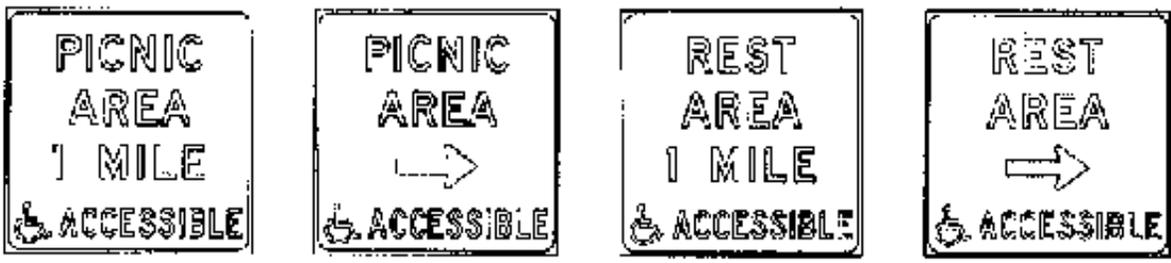


Figure 7-17. Advance and directional rest and picnic area signs (D9-6a) for conventional highways showing ADA accessible message.

### Historic Picnic Areas

Thirty-eight picnic areas around the state will not be modified to fully meet ADA standards due to their historic significance. To notify the public of the historic significance of these picnic areas and the reasons they are not fully ADA accessible, the HISTORIC ROADSIDE PARK sign (D7-10b) should be used at these sites. (See *Standard Highway Sign Designs for Texas* for [sign details](#).) The sign is shaped to resemble a 1930s Texas Highway Department ROADSIDE PARK sign. In addition to a unique message explaining the site's historical significance, the sign contains the following explanation:

In order to preserve its historic character and significance, this picnic area may not be fully accessible under the normal standards for public facilities set forth in the 1990 Americans With Disabilities Act.

The sign should be erected within the picnic area, and is not intended to be visible from the roadway.

A listing of the historic picnic areas may be obtained from the Traffic Operations Division (TRF).

# Chapter 8

## Miscellaneous Signs

### Contents:

Section 1 — Overview

## **Section 1**

### **Overview**

NOTE: The on-line version of this manual currently contains only the latest revision of Chapter 7.  
For other content, see the print version.

# Chapter 9

## Expressway and Freeway Signing

### Contents:

Section 1 — Overview

## **Section 1**

### **Overview**

NOTE: The on-line version of this manual currently contains only the latest revision of Chapter 7.  
For other content, see the print version.

# Chapter 10

## Markings and Delineators

### Contents:

Section 1 — Overview

## **Section 1**

### **Overview**

NOTE: The on-line version of this manual currently contains only the latest revision of Chapter 7.  
For other content, see the print version.

# Chapter 11

## Other Traffic Control Situations

### Contents:

Section 1 — Overview

## **Section 1**

### **Overview**

NOTE: The on-line version of this manual currently contains only the latest revision of Chapter 7.  
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