

## **Historic Bridge – Off-System Amendments**

Two-Party Amendment Form

Three-Party Amendment Form

For a current version of these forms, see

<http://crossroads/org/gsd/Contract%20Services/Standard%20Contracts/>

### **Example Exhibits**

#### **Work Performed by Recipient (Direct Funding Option)**

Exhibit A – Proposal and Plan .....2

Exhibit B – Responsibilities of the State.....6

Exhibit C – Responsibilities of the Recipient .....7

#### **Work Performed By State’s Contractor**

Exhibit A – Proposal and Plan .....8

Exhibit B – Responsibilities of the State.....11

Exhibit C – Responsibilities of the Recipient .....12



Public Works  
Department  
Construction Management,  
Engineering, Streets, Water,  
Sewer, Waste Water Treatme  
Drainage and Traffic Managem

March 20, 2000

Mr. Robert A. Appleton, P.E.  
Bryan District Director of Transportation  
Planning and Development  
Texas Department of Transportation  
1300 North Texas Avenue  
Bryan, Texas 77803-2760

RECEIVED

MAR 27 2000

DESIGN DIVISION

Re: Re-Use Plan Milam County Bridge  
Texas Department of Transportation Bridge #212

Dear Mr. Appleton:

As we discussed last week, the City of Alvin, Texas would like to acquire the Milam County Historic Bridge for relocation and reuse as a pedestrian trail bridge on the Alvin-Mustang Trail.

The bridge will provide a crossing of Mustang Bayou Slough located north of South Street on the East Side of Mustang Bayou. The crossing will provide a connection from the on street system of bikeways to the multipurpose trail that leads 1.3 miles north to the Historic Santa Fe Depot. The photographs attached show the general site conditions of the proposed crossing.

The project in which this bridge will be used is an approved and funded ISTE A Enhancement Project. Texas Department of Transportation and the City of Alvin are to construct the trails and a portion of those funds will restore the bridge to the Secretary of the Interiors Standards. The project is funded at \$998,000 and includes three crossings of the Bayou and Slough. This historic bridge provides an opportunity to save a significant structure and allow its reuse consistent with the environment and theme of the trail. Without this bridge the budget for the project would be very difficult to achieve. The alternative is to use a new truss bridge at a cost of \$1,100 per linear foot.

Based on our Engineer's recommendations, a contractor will remove and transport the structure to Alvin. A complete set of restoration plans has been prepared for that work. Included with the bid package will be a lifting plan and area maps (attached) to guide the removal process.

The successful mover will provide on-site reinforcement to stabilize the bridge members. Decking will be removed and will become the property of the contractor.

The bridge is proposed to be removed using a 6-point lift. The bedstead legs will be cut approximately 6 feet below the bottom chord and then lifting the bridge with a spreader beam to ensure a vertical lift with no racking. The bridge will be transported

Mr. Robert A. Appleton, P.E.  
March 20, 2000  
Page 2

In one piece on a suitable low-boy trailer to Alvin using Texas Department of Transportation approved routes.

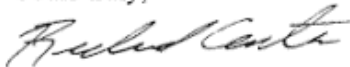
Once in Alvin it will be unloaded as close as possible to its new location and placed on blocking. Restoration funded by the enhancement project will repair or replace damaged elements and it will be cleaned and painted. The State Historical Preservation Officer will be notified of the schedule of work and will be offered the opportunity to inspect the work. During the progress of all work a Texas Department of Transportation inspector from the Angleton area office will be in residence or accessible to the project.

The measured drawings prepared for the restoration will be field verified and "as built" will be prepared for the State Historical Preservation Officer and Federal Highway Administration records.

The bridge will have new wood decking and pedestrian safety handrails. These handrails will be attached to the deck and not the bridge so its historical character will not be altered. Finally a Texas Historical Commission plaque will be applied for and erected.

As we discussed, a preliminary set of plans for the trail and for moving prepared by our consultant, Centurion Consulting Group, is enclosed. If you have questions please contact either Mr. Dexter Jones P.E. the design engineer with Centurion Consulting group at 713-242-7030 or me.

Yours truly,



Richard Carter P.E.  
Director of Public Works and Engineering

TCK/pkm/dt

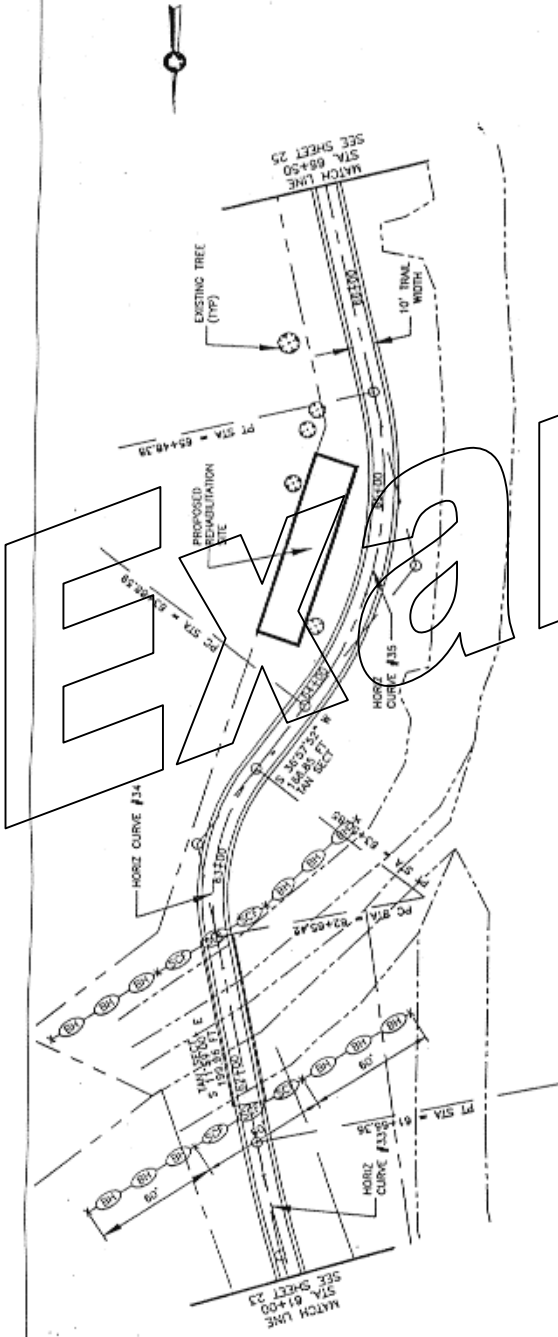
Cc: Mr. Mike O'Toole P.E.  
Mr. Dexter Jones P.E.

Attachments

**RECEIVED**  
MAR 27 2000  
DESIGN DIVISION

**HORIZONTAL CURVE DATA**

CURVE #33		CURVE #34		CURVE #35	
DA	= 07°09'43"	DA	= 47°37'40"	DA	= 31°59'52"
T	= 55.39	T	= 45.50	T	= 41.44
R	= 800.00	R	= 100.00	R	= 180.00
L	= 110.61	L	= 85.43	L	= 158.78
C	= 110.32	C	= 82.86	C	= 154.58
ED	= 1.52	ED	= 9.87	ED	= 18.31
MO	= 1.51	MO	= 8.98	MO	= 17.44
PC	= 61+11.15	PC	= 63+10.93	PC	= 64+74.18
P	= 593242.57	P	= 593046.86	P	= 592912.05
N	= 3186574.70	N	= 3186574.70	N	= 3186514.77
E	= 073519.19	E	= 48°06'52" R	E	= 50°51'44" L



**LEGEND**

- Da DEGREE OF CURVE-ARC DEFINITION
- T TANGENT LENGTH
- R CURVE RADIUS
- L CHORD LENGTH
- C EXTERNAL DISTANCE
- ED OFFSET TO CURVE
- MO MOULDER CURVE
- PC POINT OF CURVE
- P POINT OF INTERSECTION
- PI POINT OF INTERSECTION
- PIR POINT OF INTERSECTION
- E EAST
- INT INTERIOR ANGLE
- PVT POINT OF VERTICAL TANGENT
- PVI POINT OF VERTICAL INTERSECTION
- EL ELEVATION
- K RATE OF VERTICAL CURVATURE
- L TEMPORARY SEDIMENT CONTROL FENCE
- BE BAILED MAT

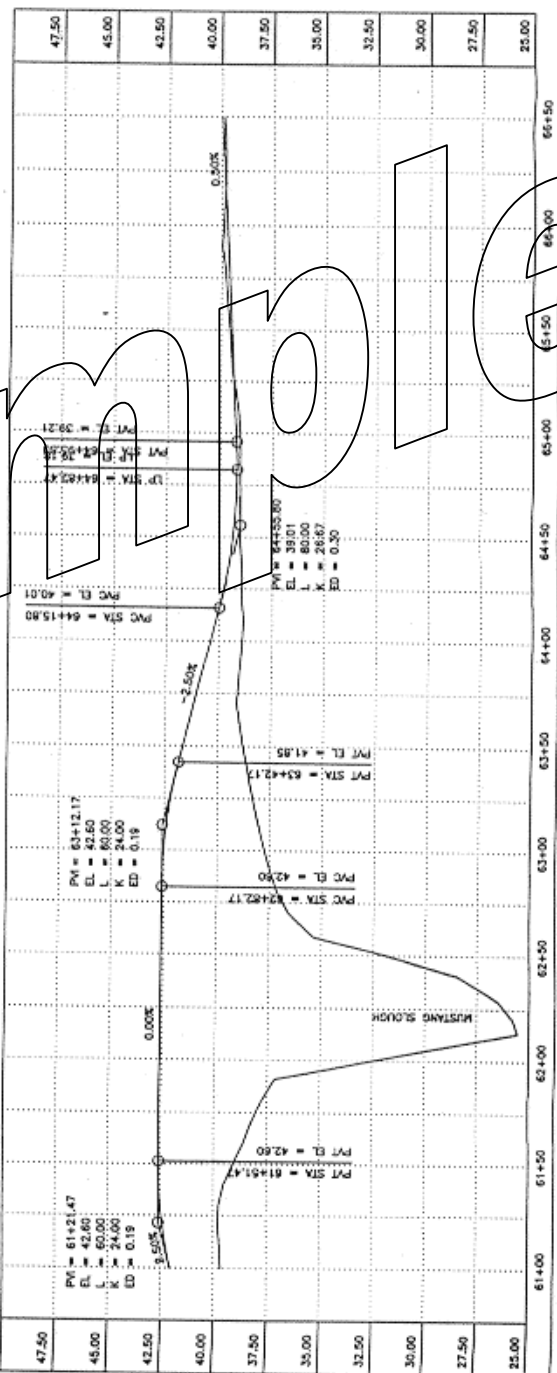
SCALES: 1" = 50' HOR  
1" = 5' VER



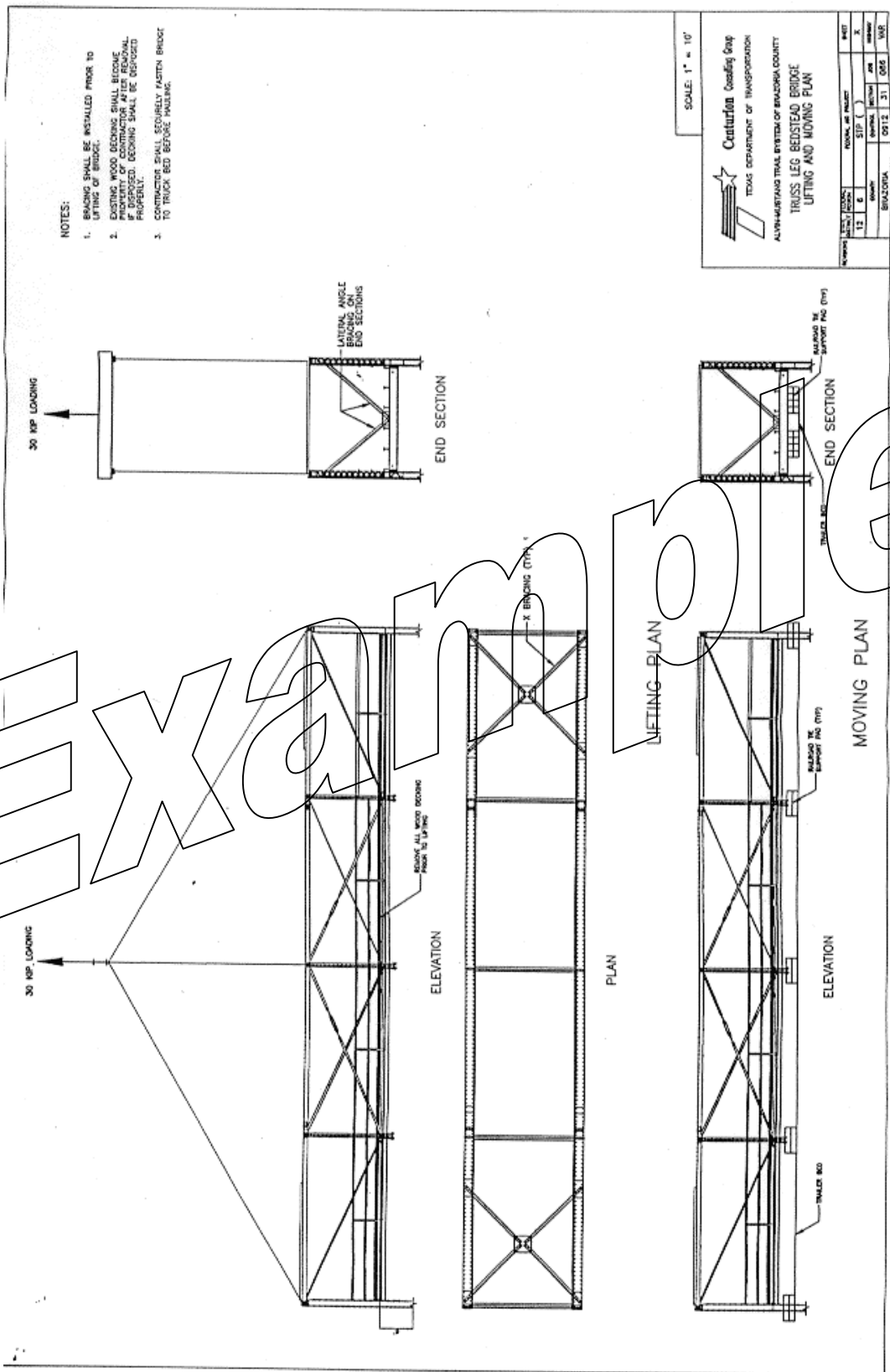
TEXAS DEPARTMENT OF TRANSPORTATION

ALBUQUERQUE TRAIL SYSTEM OF BRAZOSHA COUNTY  
MUSTANG TRAIL  
MUSTANG TRAIL  
STA 61+00 TO STA 66+50

NO. OF SHEETS	12	SHEET NO.	13 OF 14
DATE	09/11	BY	31
SCALE	1" = 50'	DATE	09/11
PROJECT NO.	0911	BY	31
DATE	09/11	BY	31
SCALE	1" = 50'	DATE	09/11
PROJECT NO.	0911	BY	31
DATE	09/11	BY	31



# EX-30



County: Milam  
CSJ: 0917-12-039  
Project: BR 94(31)OX  
Road/Street: CR 212  
NBI Structure No:  
171660AA0212002  
Local Designation No: Milam CR 119  
Feature Crossed:

## EXHIBIT B

### RESPONSIBILITIES OF THE STATE

The State will be responsible for preparing the Agreement for the Preservation and Adaptive Use of a Historic Bridge Off the State System (Three-Party).

### REIMBURSEMENT OF COSTS

As stated in the Bridge Information Package prepared by the State to market this historic bridge, the State will reimburse the Recipient for the successful removal and relocation of the historic bridge described in the Agreement for the Preservation and Adaptive Use of a Historic Bridge Off the State System (Three-Party). Once the State is satisfied that the historic bridge has been successfully removed and relocated to the site described in the Recipient's Proposal (Exhibit A), the State will reimburse the Recipient for the actual cost of the work. The State's financial responsibility for reimbursement is limited to \$20,000 as stated in the Bridge Information Package.

County: Milam  
CSJ: 0917-12-039  
Project: BR 94(31)OX  
Road/Street: CR 212  
NBI Structure No:  
171660AA0212002  
Local Designation No: Milam CR 119  
Feature Crossed:

## **EXHIBIT C**

### **RESPONSIBILITIES OF THE RECIPIENT**

The Recipient or the Recipient's Contractor will be responsible for the following items of work:

All engineering associated with the design and construction of the new foundations to ensure that they meet the appropriate design guidelines and standards.

All site preparation at the receiving site, including grading, foundations, abutments, etc.

The removal and transport of the bridge from its existing location to the receiving site. This work will include all repairs necessary to ensure the bridge is not damaged during its removal and/or transport to the receiving site.

The removal of any existing foundations and piling from the existing channel which may impede the flow of water and diminish the capacity of the channel.

All permanent repairs, including the replacement of missing and/or damaged structural members; furnishing any necessary hardware to reassemble the bridge on the new foundations; and all cleaning, priming and painting of the bridge as permitted and approved by the State Historic Preservation Officer.

Construct all necessary approach spans, including positive measures to prevent the use of the bridge by vehicular traffic, decking, handrails and other appurtenances as permitted and approved by the State Historic Preservation Officer.

The Recipient shall notify the State in writing when all work outlined above is completed in order that the State may visit the site for the sole purpose of confirming that the work has been accomplished.

### **REIMBURSEMENT OF COSTS**

The cost of performing the responsibilities listed above shall be born entirely by the Recipient with the State to reimburse the Recipient up to an amount not to exceed \$20,000. Reimbursement will be made by the State upon the receipt of a written invoice submitted by the Recipient upon the satisfactory completion of the work.



## City of Van Alstyne

August 12, 1998

Mr. Bobby G. Littlefield, P.E.  
Area Engineer  
Texas Department of Transportation  
3711 Highway 75 South  
Sherman, Texas 75490

RE: Bridge Replacement Program

Dear Mr. Littlefield,

This letter is in regard to the bridge located on North Lincoln Park Road over the West Prong of Sister Grove Creek. It is the desire of the City of Van Alstyne that this bridge be a part of our new park which is to be located in Van Alstyne on Highway 5 South by the new elementary school.

Van Alstyne has always strived to preserve it's past which is apparent by the fact that we have the oldest historical museum in Grayson County. We feel that the historical significance of this "100 plus" year old bridge will not only be an asset to our park but must be preserved for future generations.

Our park will be an excellent location for the bridge in that it will be easily accessible to the public and located where it can be enjoyed by all including numerous elementary school students. The Van Alstyne Parks and Recreation Association has agreeded to the financial support of relocating the bridge.

If further information is needed in regard to this request please feel free to contact me. Thank you for your consideration.

Sincerely,

Teddie Ann Salmon, Mayor  
City of Van Alstyne

207 E. JEFFERSON • P.O. BOX 247 • VAN ALSTYNE, TX 75495-0247 • 903 • 482 • 5426



# EXISTING LAND USE

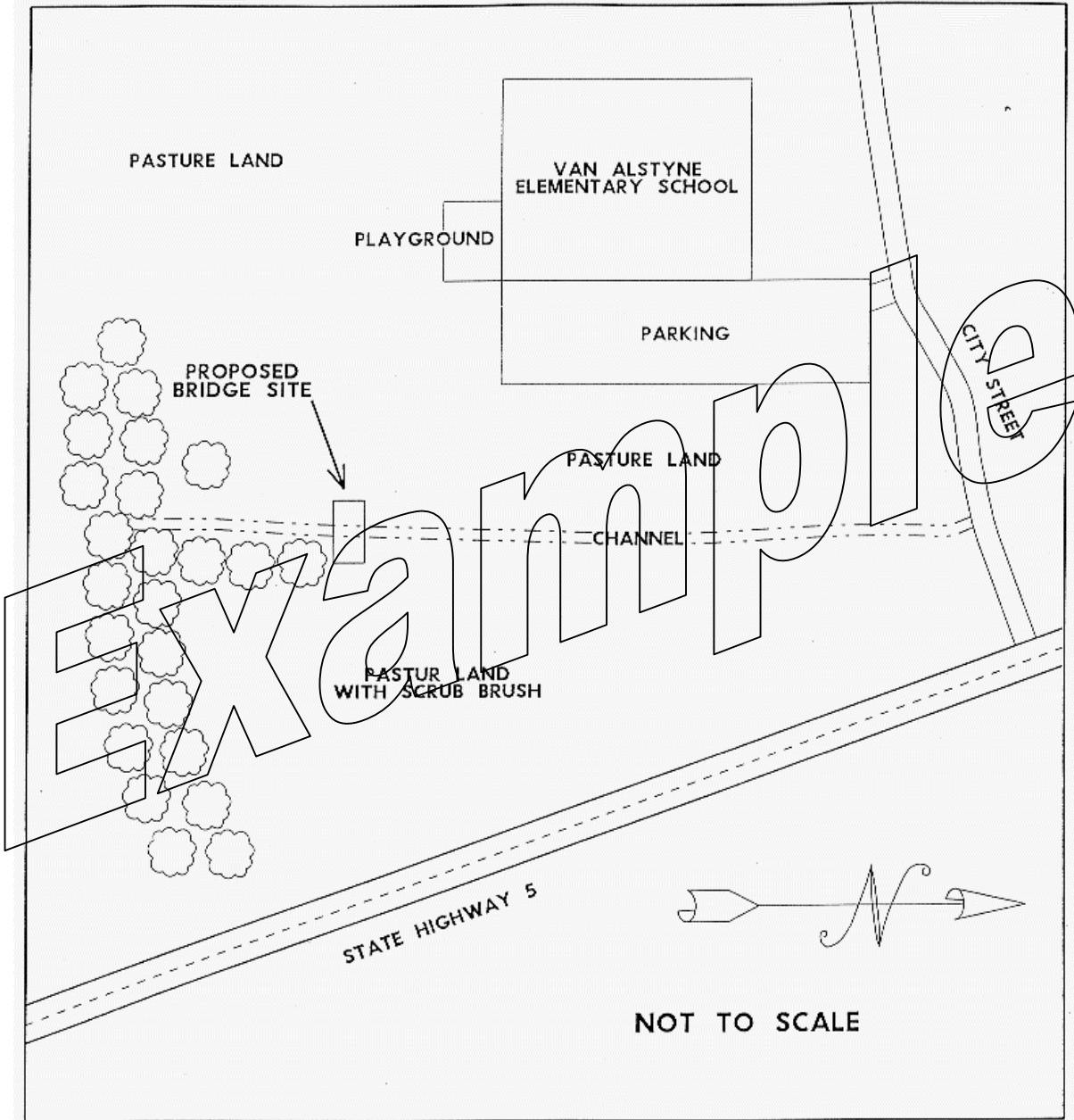


EXHIBIT A1  
PROPOSED BRIDGE PLACEMENT SITE  
CR 206 \* WEST PRONG SISTER GROVE CREEK  
GRAYSON COUNTY  
CSJ # 0901-19-105

# PROPOSED CITY PARK

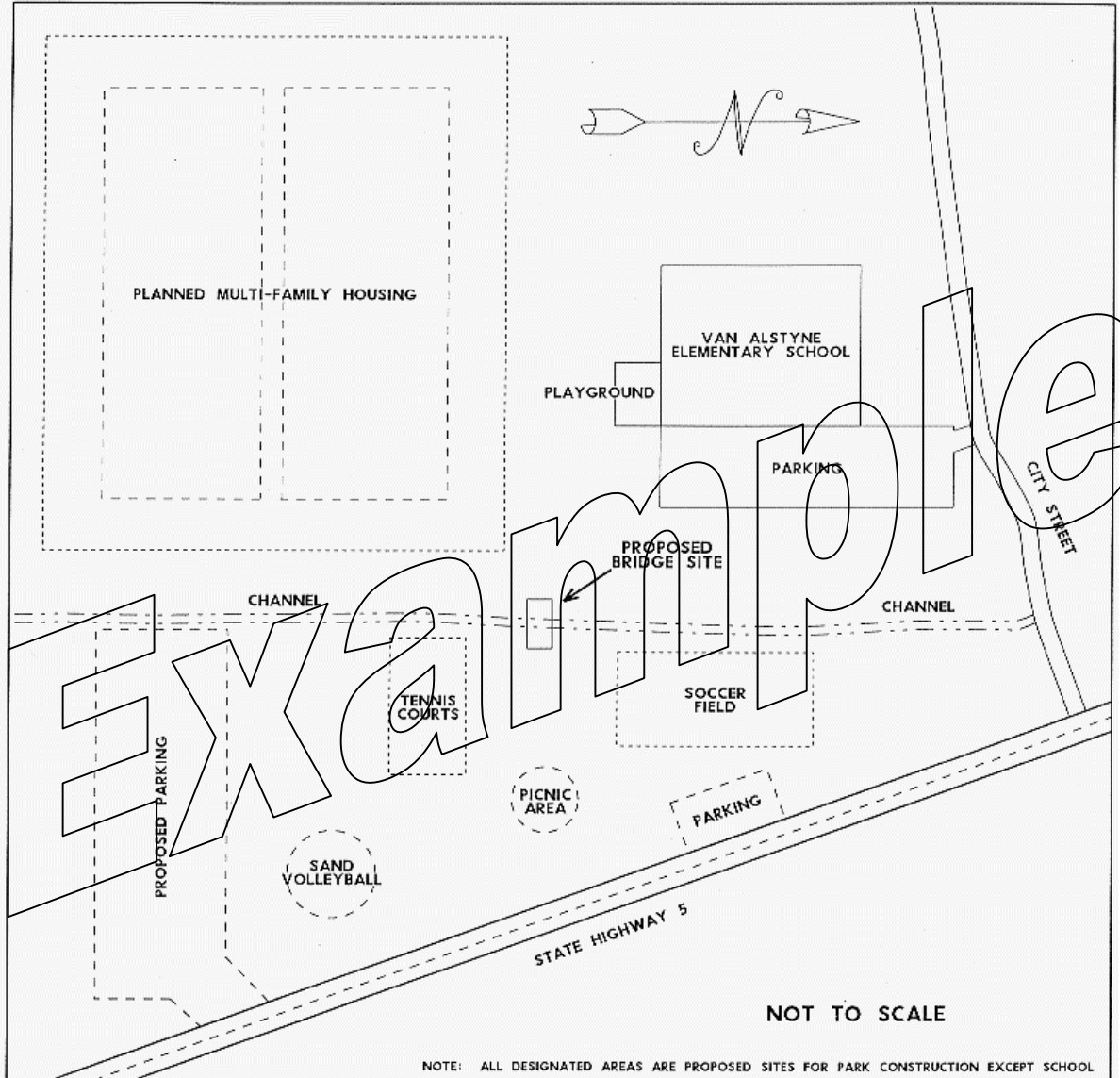


EXHIBIT A2  
PROPOSED BRIDGE PLACEMENT SITE  
CR 206 \* WEST PRONG SISTER GROVE CREEK  
GRAYSON COUNTY  
CSJ # 0901-19-105

County: Grayson  
CSJ: 0901-19 -105  
Project: BR 99 (578)  
Road/Street: CR 206  
NBI Structure No: AA0206001  
Local Designation No.:  
Feature Crossed: W Prong Sister

Grove

## EXHIBIT B

### RESPONSIBILITIES OF THE STATE

The State or the State's contractor will be responsible for the following items of work:

Removing all timber decking and timber stringers. Timber decking will become property of Van Alstyne.

Moving the truss structure intact from existing bridge abutments.

Making any repairs to the structure that the State determines to be necessary.

Cleaning and painting the structure after transporting to city park.

Transporting the truss structure to Van Alstyne City Park, adjacent to State Highway 5, in the City of Van Alstyne, Texas.

Unloading and erecting structure onto cribbing that has been placed by the Recipient to receive the structure.

Installation of interpretive plaque of historic structure adjacent to structure.

### REIMBURSEMENT OF COSTS

The cost for performing the responsibilities listed above shall be born entirely by the State, with no reimbursement to be made to the State by the Recipient for this work.

County: Grayson  
CSJ: 0901-19 - 105  
Project: BR 99(578)  
Road/Street: CR 206  
NBI Structure No: AA0206001  
Local Designation No.  
Feature Crossed: W Prong Sister Grove

## EXHIBIT C

### RESPONSIBILITIES OF THE RECIPIENT

The Recipient or the Recipient's contractor will be responsible for the following items of work:

All preparation of site plan layout details including proposed pedestrian rail approaches.

All site preparation at the receiving site, including grading, foundations, abutments, etc.

Furnishing any necessary hardware and placing the bridge on the new foundation.

Furnishing and installing stringers and decking.

Constructing all necessary approach spans, pedestrian rails and other appurtenances.

### REIMBURSEMENT OF COSTS

The cost for performing the responsibilities listed above shall be born entirely by the Recipient, with no reimbursement to be made to the Recipient by the State for this work.